



FP-002A  
(Rev. 1.2018)

The Commonwealth of Massachusetts  
City/Town of Salem, MA

**Application For License**

Massachusetts General Law, Chapter 148 §13

New License     Amended License

GIS Coordinates
<u>42.50</u>
LAT.
<u>-70.93</u>
LONG.
License Number

Application is hereby made in accordance with the provisions of Chapter 148 of the General Laws of Massachusetts for a license to store flammables, combustibles or explosives on land in buildings or structures herein described.

Location of Land: 342 Highland Avenue (Map 8, Lots 14 and 15)  
Number, Street and Assessor's Map and Parcel ID

Attach a plot plan of the property indicating the location of property lines and all buildings or structures.

Owner of Land: William H. Goldberg Family, LLC

Address of Land Owner: 7 Rantoul Street, Suite 100B, Beverly, MA 01915

Use and Occupancy of Buildings and Structures: Retail Motor Fuel and Convenience Store

If this is an application for amendment of an existing license, indicate date of original license and any subsequent amendments

Attach a copy of the current license

**Flammable and Combustible Liquids, Flammable Gases and Solids**

Complete this section for the storage of flammable and combustible liquids, solids, and gases; see 527 CMR 1.00 Table 1.12.8.50; Attach additional pages if needed. All tanks and containers are considered full for the purposes of licensing and permitting.

PRODUCT NAME	CLASS	MAXIMUM QUANTITY	UNITS gal., lbs, cubic feet	CONTAINER UST, AST, IBC, drums
<u>Gasoline</u>		<u>30,000</u>	<u>Gal</u>	<u>UST</u>
<u>Diesel</u>		<u>6,000</u>	<u>Gal</u>	<u>UST</u>
<u>Propane</u>		<u>420</u>	<u>Lbs</u>	<u>20lb cyls</u>

Total quantity of all flammable liquids to be stored: 30,000 gal

Total quantity of all combustible liquids to be stored: 6,000 gal

Total quantity of all flammable gases to be stored: 420 lbs

Total quantity of all flammable solids to be stored: n/a

**LP-gas** (Complete this section for the storage of LP-gas or propane)

Indicate the maximum quantity of LP-gas to be stored and the sizes and capacities of all storage containers. (See 527 CMR 1.00 Table 1.12.8.50)

❖ Maximum quantity (in gallons) of LP-gas to be stored in aboveground containers: 90 gal (420lb) propane

List sizes and capacities of all aboveground containers used for storage: 21ea 4.5gal/20lb cylinders

❖ Maximum quantity (in gallons) of LP-gas to be stored in underground containers: NONE UG

List sizes and capacities of all underground containers used for storage: n/a

Total aggregate quantity of all LP-gas to be stored: 90 gal (420lb) propane

**Fireworks** (Complete this section for the storage of fireworks) **NONE - NO FIREWORKS**

Indicate classes of fireworks to be stored and maximum quantity of each class. (See 527 CMR 1.00 Table 1.12.8.50)

❖ Maximum amount (in pounds) of Class 1.3G: \_\_\_\_\_ Type/class of magazine used for storage: \_\_\_\_\_

❖ Maximum amount (in pounds) of Class 1.4G: \_\_\_\_\_ Type/class of magazine used for storage: \_\_\_\_\_

❖ Maximum amount (in pounds) of Class 1.4: \_\_\_\_\_ Type/class of magazine used for storage: \_\_\_\_\_

Total aggregate quantity of all classes of fireworks to be stored: \_\_\_\_\_

**Explosives** (Complete this section for the storage of explosives) **NONE - NO EXPLOSIVES**

Indicate classes of explosive to be stored and maximum quantity of each class. (See 527 CMR 1.00 Table 1.12.8.50)

❖ Maximum amount (in pounds) of Class 1.1: \_\_\_\_\_ Number of magazines used for storage: \_\_\_\_\_

❖ Maximum amount (in pounds) of Class 1.2: \_\_\_\_\_ Number of magazines used for storage: \_\_\_\_\_

❖ Maximum amount (in pounds) of Class 1.3: \_\_\_\_\_ Number of magazines used for storage: \_\_\_\_\_

❖ Maximum amount (in pounds) of Class 1.4: \_\_\_\_\_ Number of magazines used for storage: \_\_\_\_\_

❖ Maximum amount (in pounds) of Class 1.5: \_\_\_\_\_ Number of magazines used for storage: \_\_\_\_\_

❖ Maximum amount (in pounds) of Class 1.6: \_\_\_\_\_ Number of magazines used for storage: \_\_\_\_\_

I, Anthony Guba, hereby attest that I am authorized to make this application. I acknowledge that the information contained herein is accurate and complete to the best of my knowledge and belief. I acknowledge that all materials stored pursuant to any license granted hereunder must be stored or kept in accordance with all applicable laws, codes, rules and regulations, including but not limited to Massachusetts Chapter 148, and the Massachusetts Fire Code (527 CMR 1.00). I further acknowledge that the storage of any material specified in any license granted hereunder may not exceed the maximum quantity specified by the license.

Signature  Date 10 March 2022 Name Anthony Guba, A.L. Prime Energy

*Fire Department Use Only*

I, Alan E. Diener, Head of the Salem Fire Department endorse this application with my

Approval  Disapproval

Signature of Head of the Fire Department

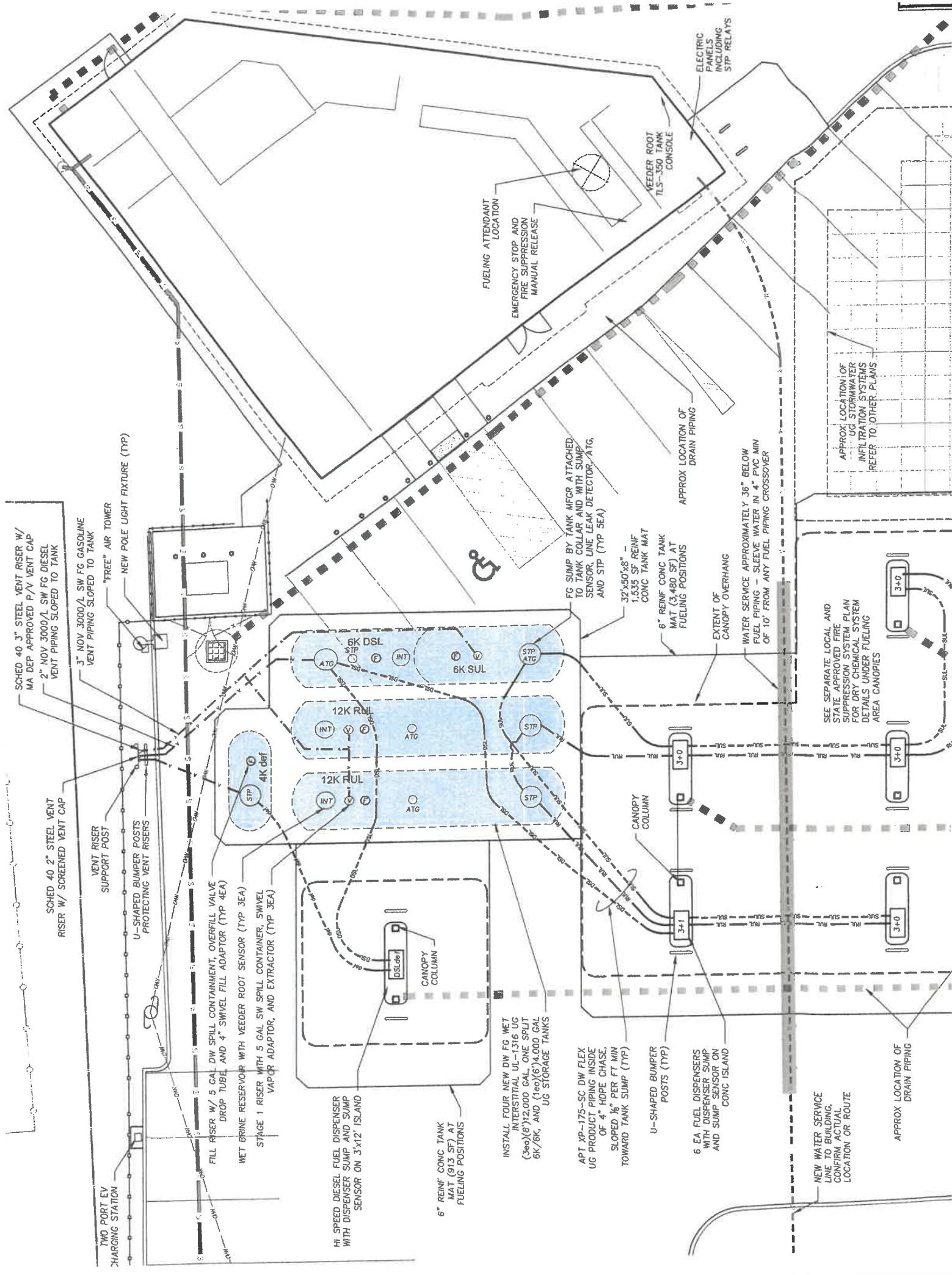
Date

Recommendations: None

**LEGEND**

1/4" = 1'00'	PROPOSED SITE IMPROVEMENTS
1/8" = 1'00'	EXISTING SITE IMPROVEMENTS
1/16" = 1'00'	PROPOSED SITE IMPROVEMENTS
1/32" = 1'00'	EXISTING SITE IMPROVEMENTS
1/64" = 1'00'	PROPOSED SITE IMPROVEMENTS
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1/256" = 1'00'	PROPOSED SITE IMPROVEMENTS
1/512" = 1'00'	EXISTING SITE IMPROVEMENTS
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1/51422015337851895053511111111111111111114143104" = 1'00'	EXISTING SITE IMPROVEMENTS
1/102844030757703790107	





SCHED 40 3" STEEL RISER W/  
MA DEP APPROVED P/V VENT CAP  
2" NOV 3000/L SW FG DIESEL  
VENT PIPING SLOPED TO TANK

SCHED 40 2" STEEL VENT  
RISER W/ SCREENED VENT CAP

TWO PORT EV  
CHARGING STATION

3" NOV 3000/L SW FG GASOLINE  
VENT PIPING SLOPED TO TANK

VENT RISER  
SUPPORT POST

FILL RISER W/ 5 GAL DW SPILL CONTAINMENT, OVERFILL VALVE  
DROP TUBE, AND 4" SWIVEL FILL ADAPTOR (TYP 4EA)

U-SHAPED BUMPER POSTS  
PROTECTING VENT RISERS

MET BRINE RESERVOIR WITH VEEDER ROOT SENSOR (TYP 3EA)  
STAGE 1 RISER WITH 5 GAL SW SPILL CONTAINER, SWIVEL  
VAPOR ADAPTOR, AND EXTRACTOR (TYP 3EA)

HI SPEED DIESEL FUEL DISPENSER  
WITH DISPENSER SUMP AND SUMP  
SENSOR ON 3'x12" ISLAND

6" REINF CONC TANK  
MAT (913 SF) AT  
FUELING POSITIONS

INSTALL FOUR NEW DW FG MET  
INTERSTITIAL UL-1316 UG  
(3ea) (8'x12,000 GAL, ONE SPLIT  
6K/6K, AND (1ea) (6'x4,000 GAL  
UG STORAGE TANKS

APT XP-175-SC DW FLEY  
UG PRODUCT PIPING INSIDE  
OF 4" HOPE CHASE,  
SLOPED 1/8" PER FT MIN  
TOWARD TANK SUMP (TYP)

U-SHAPED BUMPER  
POSTS (TYP)

6 EA FUEL DISPENSERS  
WITH DISPENSER SUMP  
AND SUMP SENSOR ON  
CONC ISLAND

NEW WATER SERVICE  
LINE TO BUILDING.  
CONFIRM ACTUAL  
LOCATION OR ROUTE

APPROX LOCATION OF  
DRAIN PIPING

APPROX LOCATION OF  
DRAIN PIPING

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DRAIN PIPING

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APPROX LOCATION OF  
DRAIN PIPING

APPROX LOCATION OF  
DRAIN PIPING

SCHED 40 3" STEEL RISER W/  
MA DEP APPROVED P/V VENT CAP  
2" NOV 3000/L SW FG DIESEL  
VENT PIPING SLOPED TO TANK

3" NOV 3000/L SW FG GASOLINE  
VENT PIPING SLOPED TO TANK

"FREE" AIR TOWER  
NEW POLE LIGHT FIXTURE (TYP)

FUELING ATTENDANT  
LOCATION

EMERGENCY STOP AND  
FIRE SUPPRESSION  
MANUAL RELEASE

FEEDER ROOT  
TLS-150 TANK  
CONSOLE

ELECTRIC  
PANELS  
INCLUDING  
STP RELAYS

APPROX LOCATION OF  
UG STORMWATER  
INFILTRATION SYSTEMS  
REFER TO OTHER PLANS

APPROX LOCATION OF  
DRAIN PIPING

FG SUMP BY TANK MFR ATTACHED  
TO TANK COLLAR AND WITH SUMP  
SENSOR, LINE LEAK DETECTOR, ATG,  
AND STP (TYP 5EA)

32'x50'x8" -  
1,535 SF REINF  
CONC TANK MAT

6" REINF CONC TANK  
MAT (3,480 SF) AT  
FUELING POSITIONS

EXTENT OF  
CANOPY OVERHANG

WATER SERVICE APPROXIMATELY 36" BELOW  
FUEL PIPING - SLEEVE WATER IN 4" PVC MIN  
OF 10' FROM ANY FUEL PIPING CROSSOVER

SEE SEPARATE LOCAL AND  
STATE APPROVED FIRE  
SUPPRESSION SYSTEM PLAN  
FOR DRY CHEMICAL SYSTEM  
DETAILS UNDER FUELING  
AREA CANOPIES

3+0

3+0

3+0

3+0

3+0

3+0

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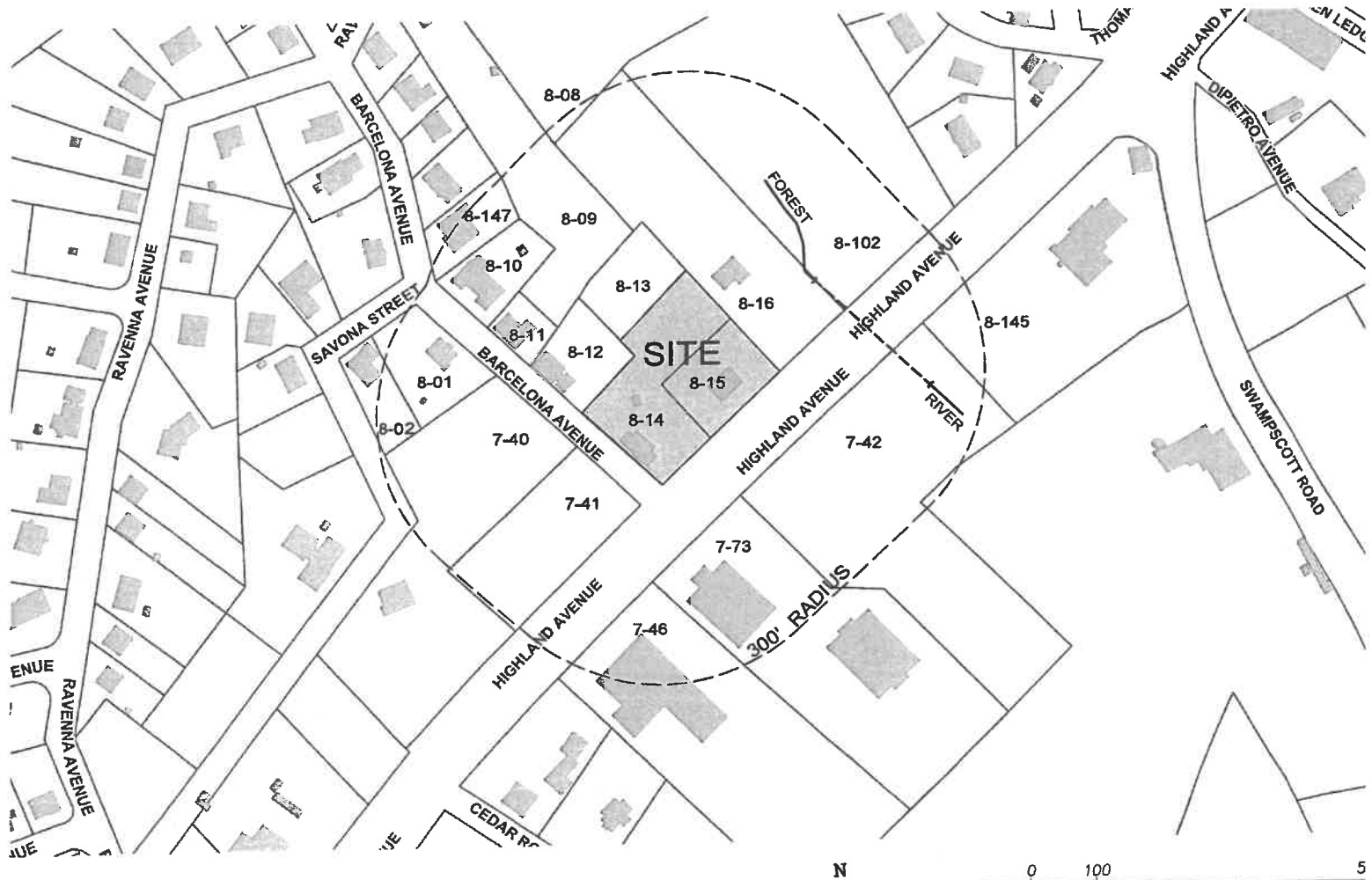
3+0

3+0

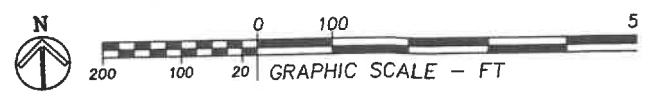
3+0

3+0

3+0



**UNITY MAP**



## Ilene Simons

---

**From:** Anthony Guba <anthony.guba@alprime.com>  
**Sent:** Tuesday, April 12, 2022 4:32 PM  
**To:** Ilene Simons  
**Subject:** RE: INFLAMMABLES AD - KEEP OR STORE FOR PUBLIC HEARING

**Caution:** This email originated from outside our system. Please do not click links or open attachments unless you recognize the sender and know the content is safe.

Good Afternoon Ilene Simons,

The motor fuel will all be underground – 30,000 Gasoline plus 6,000 Diesel = **36,000 gal total underground**

The propane will all be aboveground – 21ea 20lb cylinders (approx. 4.5gal per cylinder) = **94.5 gal total aboveground**

Thanks,  
Tony  
512-745-6400

---

**From:** Ilene Simons [mailto:[ISimons@Salem.com](mailto:ISimons@Salem.com)]  
**Sent:** Tuesday, April 12, 2022 12:05 PM  
**To:** Anthony Guba  
**Subject:** RE: INFLAMMABLES AD - KEEP OR STORE FOR PUBLIC HEARING

How many gallons are above ground and how many underground?

Ilene Simons  
City Clerk  
Justice of the Peace  
93 Washington Street  
Salem, MA 01970  
(978) 745-9595 ext. 41201

**From:** Anthony Guba <[anthony.guba@alprime.com](mailto:anthony.guba@alprime.com)>  
**Sent:** Monday, April 11, 2022 4:50 PM  
**To:** Ilene Simons <[ISimons@Salem.com](mailto:ISimons@Salem.com)>  
**Subject:** RE: INFLAMMABLES AD - KEEP OR STORE FOR PUBLIC HEARING

**Caution:** This email originated from outside our system. Please do not click links or open attachments unless you recognize the sender and know the content is safe.

Good Day Ilene Simons,

I will get original hard copies to you with returned cards. In the meantime, here is a scan of the mailing receipts for notices expected to be delivered on or about April 16<sup>th</sup>. Also attached are the notice and plan that was sent to each abutter.

ORDER FOR CONDUIT & POLE LOCATION

In the City Council for the City of Salem, Massachusetts.

ORDERED:

That permission be and hereby is granted to Comcast Cable Communications Management LCC., to lay and maintain underground conduits, manholes and poles, with the wires and cables to be placed therein, under and above the surface of the following public way or ways as requested in petition of said Company dated March 29, 2022

High Street: Locating the existing Comcast Conduit in the sidewalk on High Street, that is servicing number 50 Margin Street. Excavating to place a 17"x 30" vault over the conduit. From the newly placed vault excavating across High Street 27'+/- to place (1) 2" PVC Conduit to number 36 Margin Street. Substantially as shown on plan, filed with said petition.

Also that permission be and hereby is granted said Comcast to lay and maintain underground conduits, manholes, cables and wires in the above or intersecting public ways for the purpose of making connections with such poles and buildings as it may desire for distributing purposes.

The foregoing permission is subject to the following conditions:

1. The conduits and manholes shall be of such materials and construction and all work done in such manner as to be satisfactory to the City Council or to such officers as it may appoint to the supervision of the work.
2. Said Company shall indemnify and save the City harmless against all damages, costs and expense whatsoever to which the City may be subjected in consequence of the acts or neglect of said Company, its agents or servants, or in any manner arising from the rights and privileges granted it by the City.
3. In addition said Company shall, before a public way is disturbed for the laying of its wire or conduits, execute its bond in a penal sum of Fifty Thousand Dollars (\$50,000) (reference being had to the bond already on file with said City) conditioned for the faithful performance of its duties under this permit.
4. Said Company shall comply with the requirements of existing by-laws and such as may hereafter be adopted governing the construction and maintenance of conduits and wires, so far as the same are not inconsistent with the laws of the Commonwealth.

I hereby certify that the foregoing order was adopted at a meeting of the City Council for the City of Salem, Massachusetts, held on the \_\_\_\_\_ day of \_\_\_\_\_ 2022.

(over)

\_\_\_\_\_  
City Clerk

We hereby certify that on \_\_\_\_\_, 2022, at \_\_\_\_\_ o'clock \_\_\_\_\_ M., at Salem, Massachusetts a public hearing was held on the petition of the Comcast for permission to lay and maintain underground conduits, manholes and connections, with the wires and cables to be placed therein, described in the order herewith recorded, that we mailed at least seven days before said hearing a written notice the time and place of said hearing to each of the owners of real estate determined by the last preceding assessment for taxation along the ways parts of ways upon which the Company is permitted to construct the lines said Company under said order. And that thereupon said order was duly adopted.

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Salem City Council; Salem, Massachusetts

**CERTIFICATE**

I hereby certify that the foregoing is a true copy of a location order, and certificate of hearing with the notice adopted by the City Council for the City of Salem, Massachusetts, on the \_\_\_\_\_ day of \_\_\_\_\_ 2022, recorded with the records of location orders of said City, Book \_\_\_\_\_, Page \_\_\_\_\_. This certified copy is made under the provision of Chapter 166 of General Laws and any additions thereto or amendments thereof.

Attest:

\_\_\_\_\_  
City Clerk

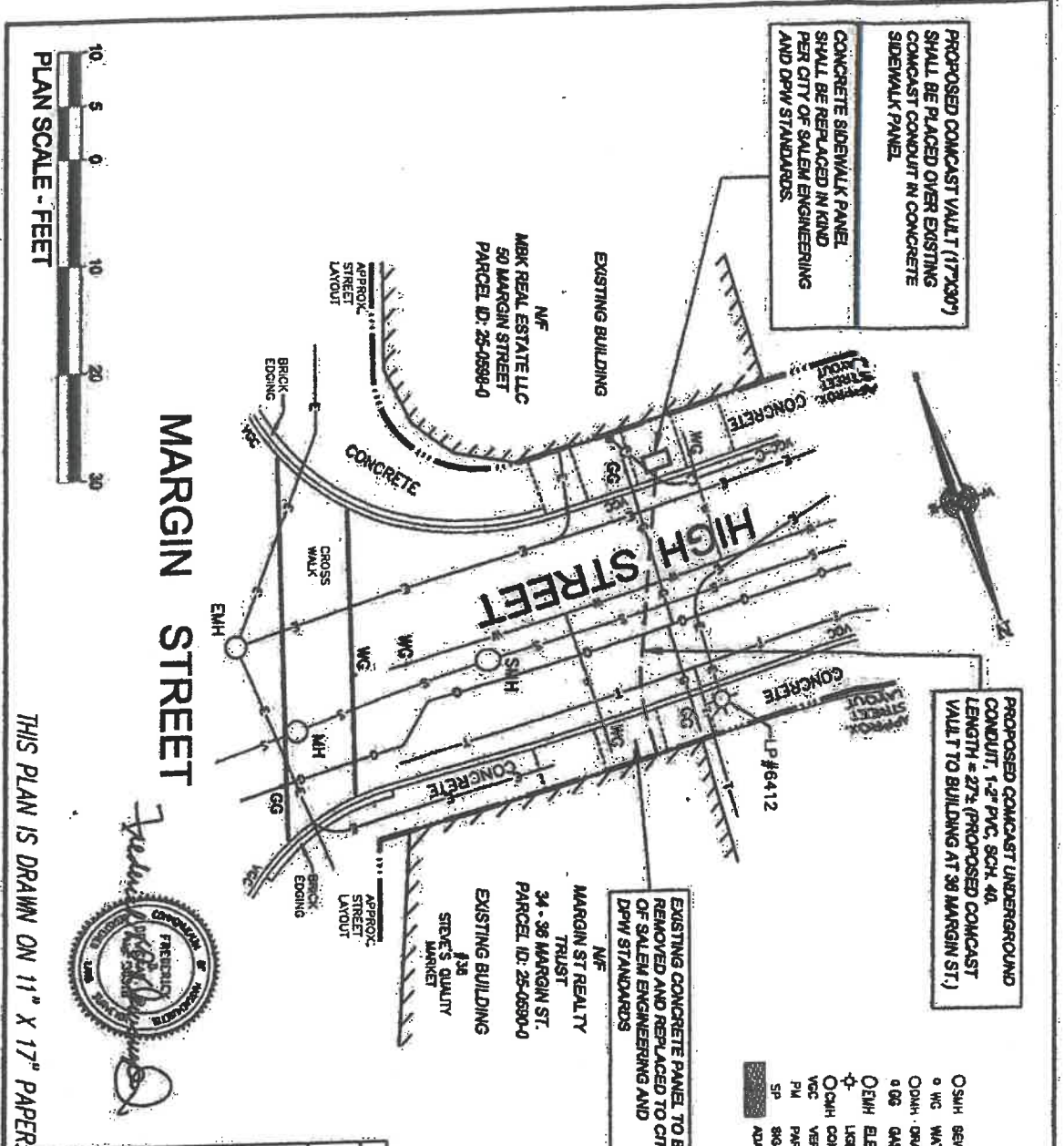


PROPOSED COMCAST VALLT (177307)  
SHALL BE PLACED OVER EXISTING  
COMCAST CONDUIT IN CONCRETE  
SIDEWALK PANEL

CONCRETE SIDEWALK PANEL  
SHALL BE REPLACED IN KIND  
PER CITY OF SALEM ENGINEERING  
AND DPW STANDARDS.

PROPOSED COMCAST UNDERGROUND  
CONDUIT, 1-2" PVC, SCH. 40,  
LENGTH = 27' (PROPOSED COMCAST  
VALLT TO BUILDING AT 38 MARGIN ST.)

EXISTING CONCRETE PAVEMENT TO BE  
REMOVED AND REPLACED TO CITY  
OF SALEM ENGINEERING AND  
DPW STANDARDS



# MARGIN STREET

THIS PLAN IS DRAWN ON 11" X 17" PAPER.



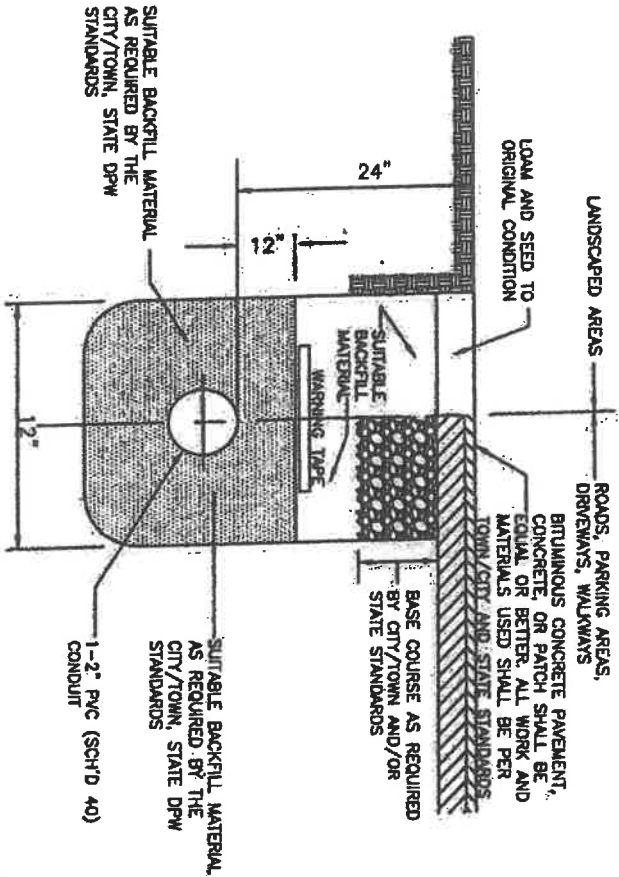
## GENERAL LEGEND

- OSMH SEWER MANHOLE
- WG WATER GATE
- ODM DRAIN MANHOLE
- OG GAS GATE
- OEMH ELECTRIC MANHOLE
- LP LIGHT POLE
- OCMH COMCAST MANHOLE
- VGC VERT. GRANITE CURB
- PM PARKING METER
- SP SIGN POST
- ADA HANDICAP
- C COMCAST UNDERGROUND CONDUIT
- DRAIN LINE
- Gas LINE
- WATER LINE
- SEWER LINE
- UNDERGROUND ELECTRIC LINE
- UNDERGROUND TELEPHONE
- PROPOSED COMCAST UNDERGROUND CONDUIT(S)

<b>WARD 2, PRECINCT 2</b>	
<b>PROPOSED COMCAST UNDERGROUND</b>	
<b>ESSEX COUNTY</b>	
<b>PLAN OF LAND</b>	
<b>IN</b>	
<b>SALEM, MA</b>	
Prepared for:	COMCAST 8 ROGERS ROAD, SUITE #8 WOBURN, MA 01801
Prepared by:	DEWISNAP ENGINEERING ASSOC. LLP 173 Lincoln Avenue - Salem, MA 01970 Tel: (781) 233-0595
Date:	MARCH 26, 2022
Checked By:	F.D.D. & P.A.D.
Drawn By:	P.A.D.
Field By:	P.A.D. & A.C.D.
Sheet No. 1 of 3	PROJECT LOCATION: 38 MARGIN STREET
	Scale: 1" = 10'

**DETAIL NOTE:**

1. The details depicted are for general reference only. The final product used shall be the responsibility of the general contractor and shall be of equal or greater material than that depicted and shall conform to the Engineering/DPW Standards for the state, city/town where the project is located.



**GENERAL CATV TRENCH DETAIL FOR 2" PVC, SCH. 40 CONDUIT**  
NOT TO SCALE

THIS PLAN IS DRAWN ON 11" X 17" PAPER.



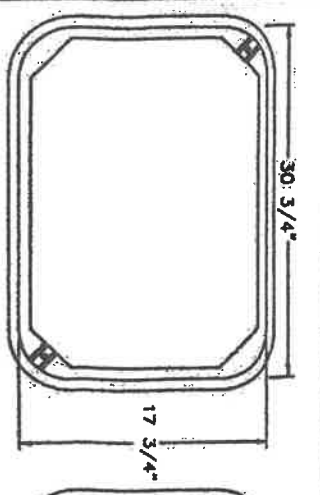
**NOTES:**

1. These plans were prepared from record plan data on file at the City of Salem, Registry of Deeds and various utility companies.
2. The locations of underground utilities shown are based on above ground structures and record drawings, if any, provided to the Surveyor. The Locations of underground utilities/structures may vary from location hereon and are not warranted to be accurate and/or correct. Additional buried utilities/structures may be encountered. No excavations were performed during the progress of this survey to locate buried utilities/structures.
3. Prior to any construction the Contractor shall notify DIG-SAFE at least 72 hours in advance at 811 (National Call Number) for verification of utilities and for field locations.
4. It is the responsibility of the Utility Contractor installing the Comcast underground conduit to notify those utility companies not associated with the DIG-SAFE system to verify the locations of their respective utilities. Also, the installing contractor shall be responsible for repairing or replacing any traffic signal loops damaged during the installation of the new Comcast underground conduit.
5. All work to be performed is the installation of a Comcast underground conduit and 17"x30" vault.
6. All work shall conform to the approving authorities Engineering and DPW Standards.
7. Prior to the start of any construction, the Utility Contractor shall confirm the number, vaults, the number of conduits and the size of the conduit(s) for this project with the Comcast project Coordinator.
8. Upon completion of the trench work for the day, the contractor shall backfill, compact and pave the trench and the work area shall be broom swept clean. In grassed areas the trench shall be loamed and seeded and hay mulch spread to keep the area stabilized until the grass has taken hold.
9. Street/Property lines are not the result of a boundary survey and are considered to be approximate.

**CONSTRUCTION NOTES:**

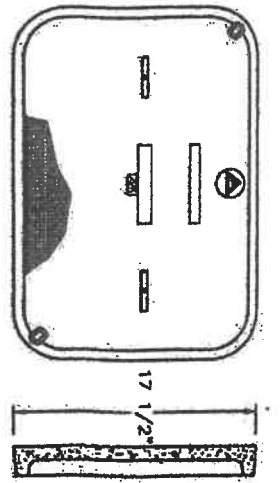
1. Following the Dig-Safe "mark out" and field verification of the existing underground utilities, the Proposed 4"x4" Manhole shall be adjusted in the field by the utility contractor to avoid being located over any existing utilities.

<b>WARD 2, PRECINCT 2</b>	
<b>PROPOSED COMCAST UNDERGROUND</b>	
<b>ESSEX COUNTY</b>	
<b>PLAN OF LAND</b>	
<b>IN</b>	
<b>SALEM, MA</b>	
Prepared for:	Prepared by:
COMCAST 9 ROGERS ROAD, SUITE 80 WOBURN, MA 01897	DEWISWP ENGINEERING ASSOC. LLP 178 UMass Avenue - Salem, MA 01970 Tel: (978) 253-0555
Date:	Scale:
MARCH 26, 2022	Not To Scale
Checked By:	PROJECT LOCATION:
F.A.D. & P.A.D.	30 MARGIN STREET
Drawn By:	
P.A.D.	
Field By:	
P.A.D. & A.C.D.	
Sheet No. 2 of 3	

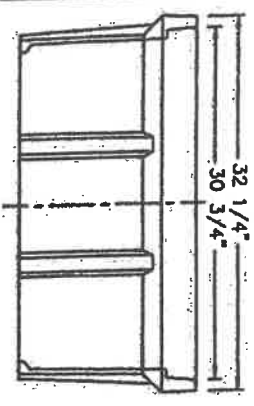
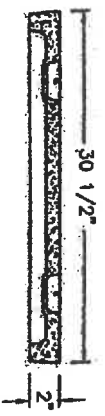


PLAN VIEW

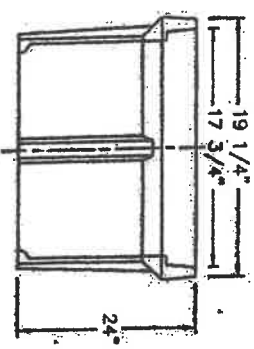
APPROX. WT. = 190 LBS.



POLYMER CONCRETE COVER



SIDE VIEW



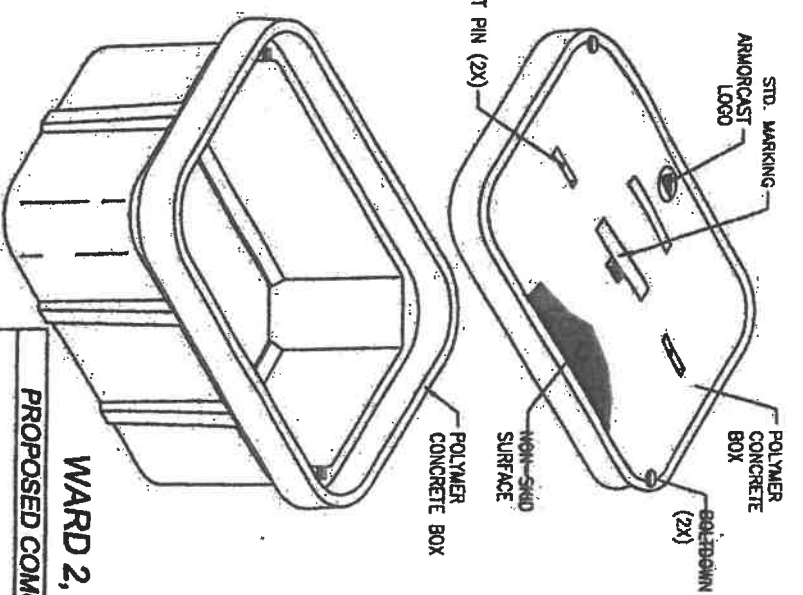
END VIEW

**ARMORCAST 17" X 30" VAULT DETAIL**

NOT TO SCALE

**VAULT NOTES:**

1. VAULTS SHALL BE AS MANUFACTURED BY ARMORCAST PRODUCTS COMPANY, CHASTWORTH, CA, 17" X 30" X 24" DEEP, TIER 22, 20K LOAD RATED, MODEL #A6001640HDAPCX24



OPEN BASE

**WARD 2, PRECINCT 2**

**PROPOSED COMCAST UNDERGROUND**

**ESSEX COUNTY**

**PLAN OF LAND**

**SALEM, MA**

Prepared For:	COMCAST 8 ROBBER HOLLOW STREET 88 WOBURN, MA 01897	Prepared By:	DEVSNAP ENGINEERING ASSOC. LLP 178 Lincoln Avenue - Salem, MA 01968 Tel: (978) 233-0585
Date:	MARCH 25, 2012	Scale:	Not To Scale
Checked By:	F.S.D. & P.A.D.	PROJECT LOCATION:	36 MARGIN STREET
Drawn By:	P.A.D.		
Field By:	P.A.D. & A.C.D.		
	Sheet No. 3 of 9		



THIS PLAN IS DRAWN ON 11" X 17" PAPER.

ORDERED:

Notice having been given and public hearing held, as provided by law, that National Grid be and it is hereby granted permission to excavate the public right of way to install the new station as shown in the plan named "4" Regulator Station Replacement 100 PSIG to 60 PSIG Liberty Hill Ave @ Appleton St," under and across the public way hereinafter named as said Company may deem necessary, in public way or ways hereinafter referred to, as requested in petition with the said COMPANY dated, April 8, 2022.

Said gas pressure regulator station shall be located substantially in accordance with the attached plan named - "4" Regulator Station Replacement 100 PSIG to 60 PSIG Liberty Hill Ave @ Appleton St."

The following are public ways along which the gas pressure regulator station above referred to may be installed: the shoulder of the roadway on Appleton St on City Property, Salem, Ma.

I hereby certify that the foregoing order was adopted at a meeting of the .....  
.....  
....., held on the ..... day of.....,20.....  
....., ..... 20.....

Received and entered in the records of location orders of the City of Salem, Ma.

Book.....Page.....

Attest:

.....hereby certify that on .....20....., at ..... o'clock,..... PM  
At....., a public hearing was held on the petition of  
NATIONAL GRID for permission to install gas pressure regulator station as described in the  
order herewith recorded, and that I mailed at least seven days before said hearing a written notice  
of the time and hearing to each of the owners of real estate (as determined by the last preceding  
assessment for taxation) along the ways or parts of ways upon which the Company is permitted  
to install gas pressure regulator station under said order. And that thereupon said order was duly  
adopted.

.....  
.....  
.....



# nationalgrid

**4" REGULATOR STATION REPLACEMENT**  
**100 PSIG TO 60 PSIG**  
**LIBERTY HILL AVE @ APPLETON ST,**  
**SALEM, MA**  
**W.O. NO.: 90000217367**  
**STATION 589**



LOCATION MAP  
 LOCATION: 42°41'N 70°11'W  
 SCALE: 1:500

SHEET	NAME	TITLE
1	SR-SAL-90000217367 G-001	COVER - LOCATION MAP
2	SR-SAL-90000217367 G-002	CONSTRUCTION NOTES
3	SR-SAL-90000217367 G-003	CONSTRUCTION NOTES
4	SR-SAL-90000217367 E-001	EXISTING CONDITIONS
5	SR-SAL-90000217367 E-002	EXISTING CONDITIONS
6	SR-SAL-90000217367 C-008	EXISTING CONDITIONS
7	SR-SAL-90000217367 C-004	PROPOSED CONDITIONS
8	SR-SAL-90000217367 C-005	TR-FM DETAILS
9	SR-SAL-90000217367 C-006	WELT TR-FM DETAILS
10	SR-SAL-90000217367 C-007	ASPH/CONCRETE
11	SR-SAL-90000217367 C-008	ASPH/CONCRETE
12	SR-SAL-90000217367 M-001	DETAILS
13	SR-SAL-90000217367 M-002	DETAILS
14	SR-SAL-90000217367 X-001	BILL OF MATERIALS



NO.	DATE	DESCRIPTION	BY	CHECKED
1		DESIGN		
2		CONSTRUCTION		
3		ASPH/CONCRETE		
4		DETAILS		
5		DETAILS		
6		DETAILS		
7		DETAILS		
8		DETAILS		
9		DETAILS		
10		DETAILS		
11		DETAILS		
12		DETAILS		
13		DETAILS		
14		DETAILS		

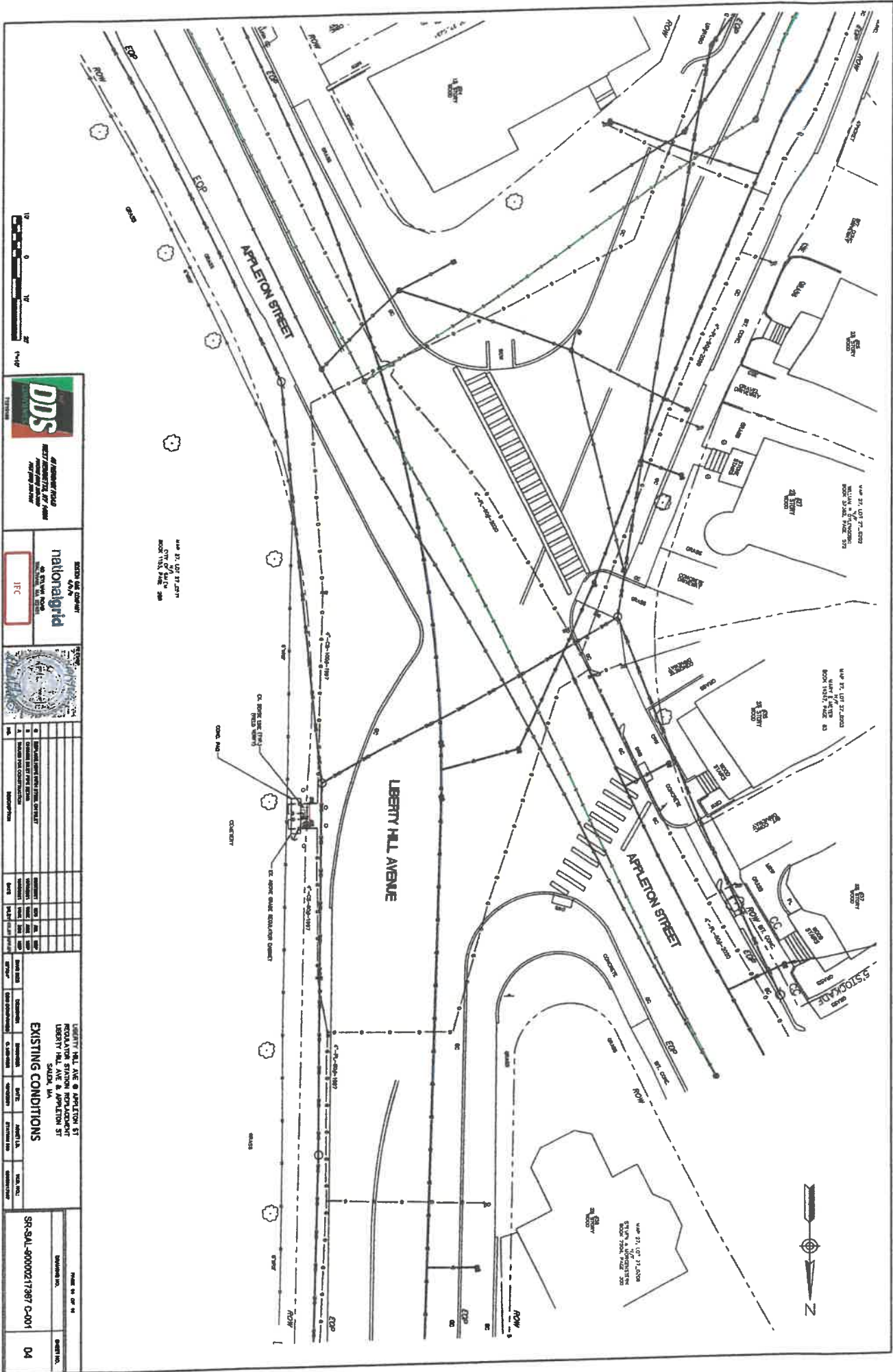
LIBERTY HILL AVE @ APPLETON ST  
 REGULATOR STATION REPLACEMENT  
 LIBERTY HILL AVE @ APPLETON ST  
 SALEM, MA  
**COVER - LOCATION MAP**

DATE: 01/14/2024  
 DRAWN BY: [Name]  
 CHECKED BY: [Name]  
 SR-SAL-90000217367 G-001  
 SHEET NO. 01









DESIGNED BY  
**nationalgrid**  
 10000 W. 10th Ave.  
 Golden, CO 80401  
 303.440.1000



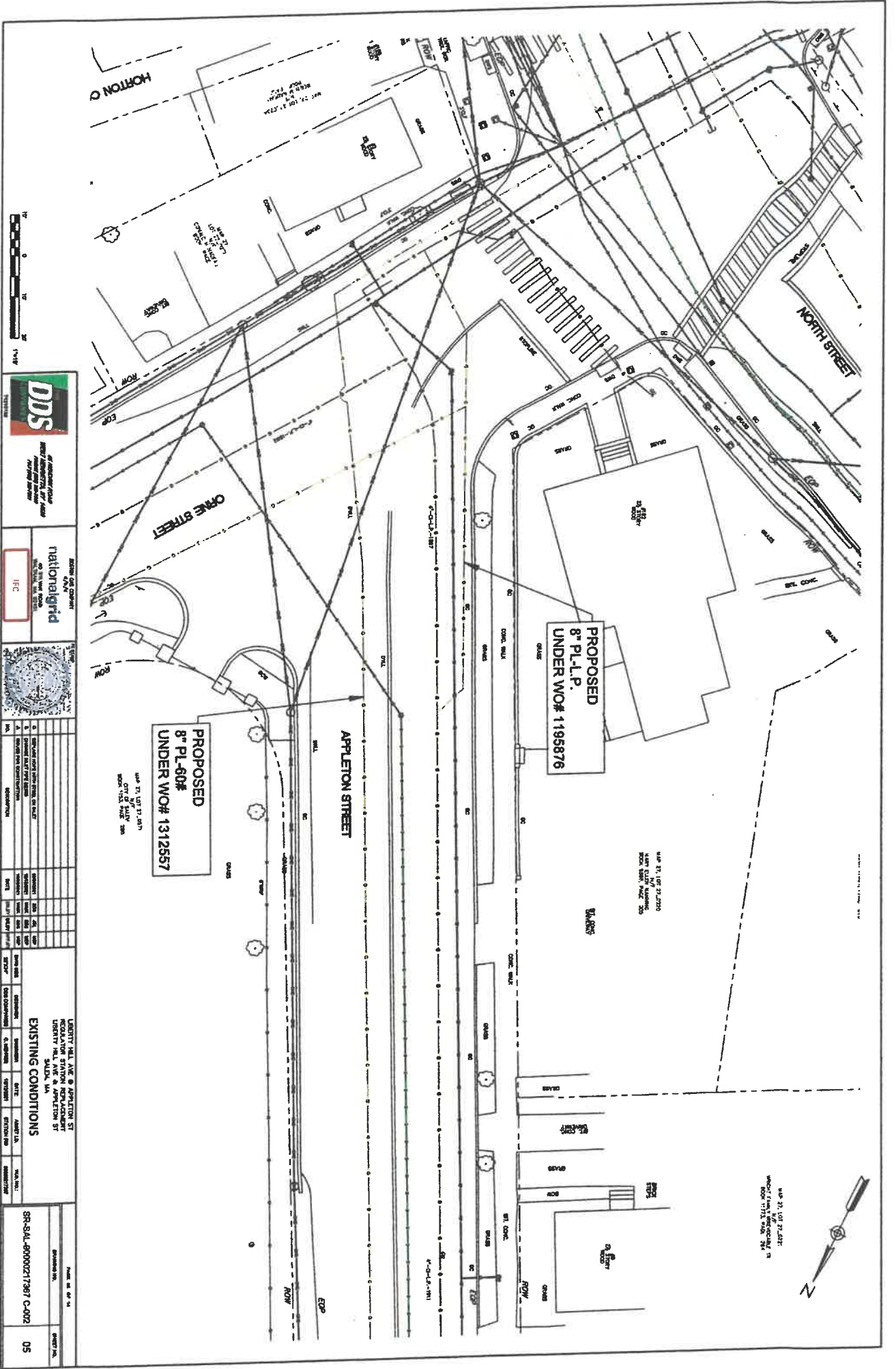
NO.	DESCRIPTION	DATE	BY	CHECKED	APP'D
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2	CHECKED	08/14/12	DJS		
3	APPROVED	08/14/12	DJS		

NO.	DESCRIPTION	DATE	BY	CHECKED	APP'D
1	DESIGNED	08/14/12	DJS		
2	CHECKED	08/14/12	DJS		
3	APPROVED	08/14/12	DJS		

LIBERTY HILL AVE @ APPLETON ST  
 REGULATORY STATION MANAGEMENT  
 LIBERTY HILL AVE & APPLETON ST  
 SADDLE VALLEY, CA  
**EXISTING CONDITIONS**

SR-SAL-00000217987 C-001  
 SHEET NO. 04





DESIGN DEVELOPMENT SERVICES, INC.  
nationalcityrid  
10000 W. 10TH AVENUE, SUITE 100  
DENVER, CO 80202  
303.751.1100  
JIC



NO.	DESCRIPTION	DATE	BY	CHKD.	APP'D.
1	DESIGNED AND PREPARED FOR CONSTRUCTION	08/11/11	JIC		
2	REVISIONS				
3	REVISIONS				
4	REVISIONS				
5	REVISIONS				
6	REVISIONS				
7	REVISIONS				
8	REVISIONS				
9	REVISIONS				
10	REVISIONS				

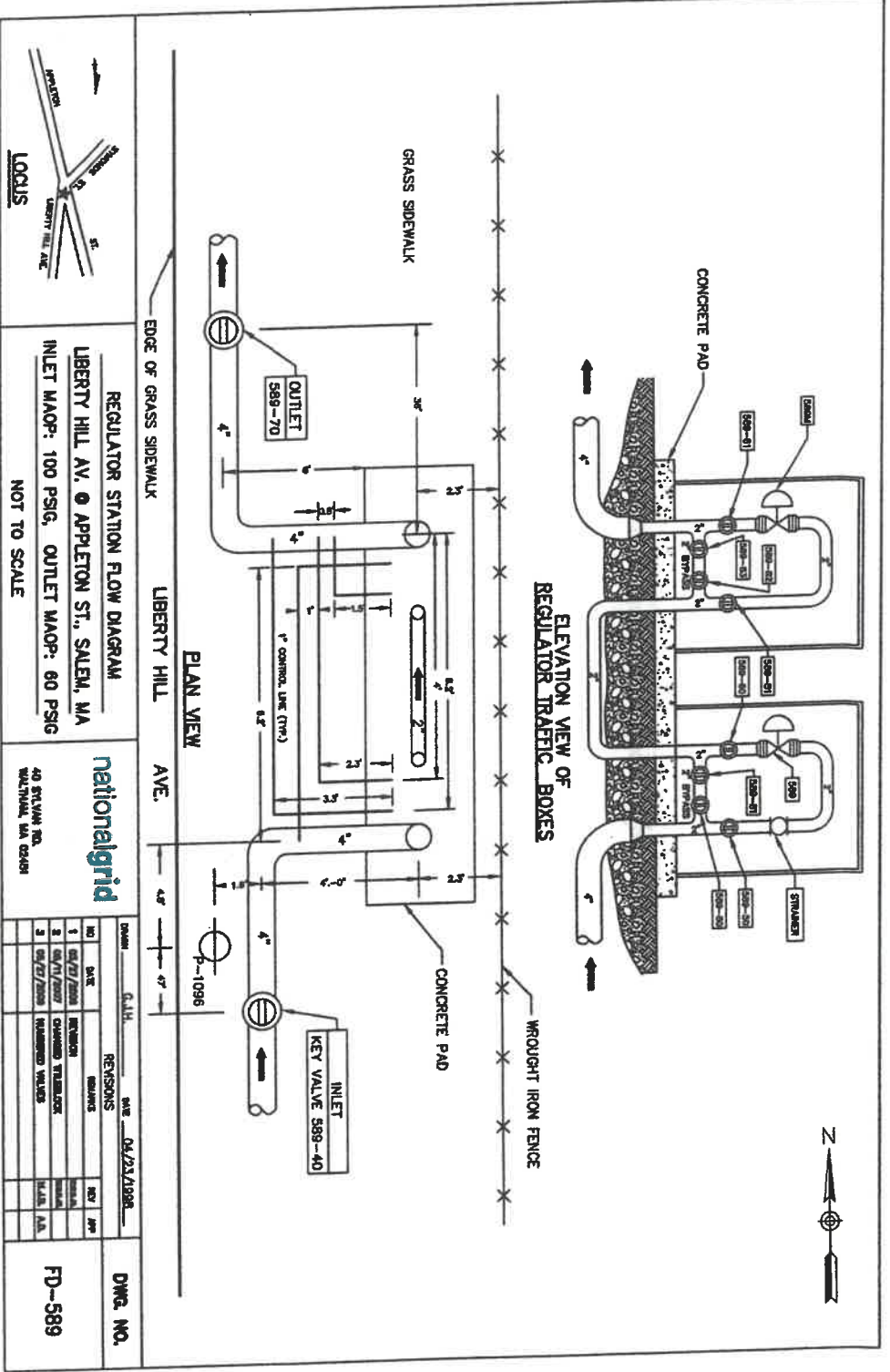
LIBERTY HALL AVE @ APPLETON ST  
LIBERTY HALL AVE @ APPLETON ST  
SAULSBURY, IA  
EXISTING CONDITIONS  
DATE: 08/11/11  
SCALE: AS SHOWN

SR-SAL-890002717987 C-002  
05

MAP 21, 101 21.037  
UNOFFICIAL IN  
BOOK 171A, PAGE 24



NOTE:  
1. EXISTING CONDITIONS FLOW DIAGRAM



nationalgrid  
40 STUYVA RT.  
METHUEN, MA 02454



NO.	DATE	REVISIONS	BY	APP.
1	04/23/1998	ISSUED FOR PERMITS		
2	04/23/1998	REVISED FOR PERMITS		
3	04/23/1998	REVISED FOR PERMITS		

LIBERTY HILL AVE @ APPLETON ST  
LIBERTY HILL AVE & APPLETON ST  
SALEM, MA

**EXISTING CONDITIONS**

SR-SAL-000021787 C-003	06
------------------------	----

NOTE:  
 1. ALL CURBING, PAVEMENT, SIDEWALKS, LAWN  
 AREAS, STRIPING, DRIVEWAYS, TRAFFIC SIGNALS  
 LOOPS, FENCES, ETC. MUST BE RESTORED IN KIND  
 AND/OR TO MUNICIPAL STANDARDS.



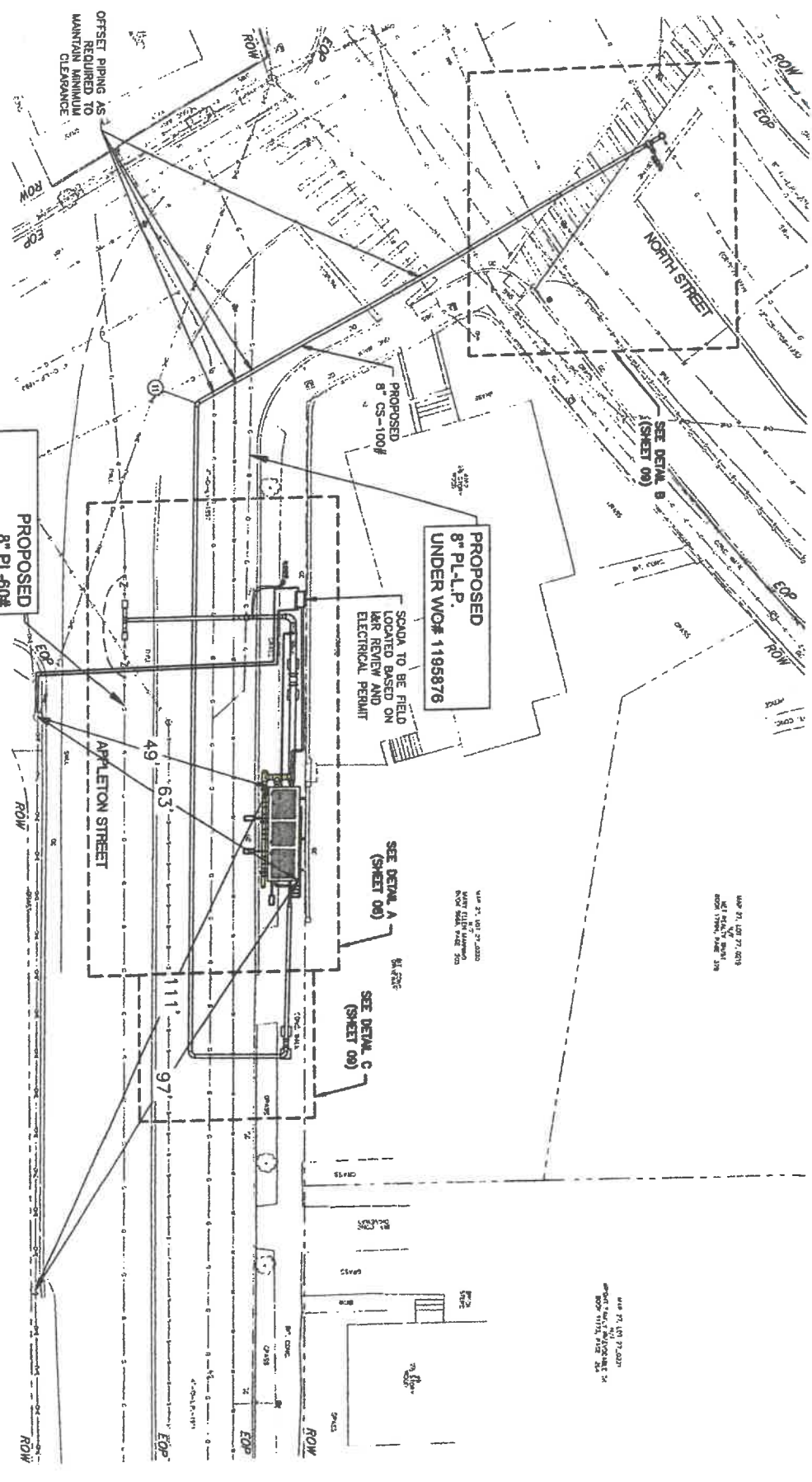
SEVEN AND COMPANY  
 nationalgrid  
 IFC



NO.	DESCRIPTION	DATE	BY	CHECKED
1	ISSUED FOR PERMITTING			
2	ISSUED FOR CONSTRUCTION			
3	ISSUED FOR AS-BUILT			
4	ISSUED FOR RECORD			

LIBERTY HILL, INC. @ APPLETON ST  
 REGULATORY STATION REPLACEMENT  
 LIBERTY HILL, INC. @ APPLETON ST  
 PROPOSED CONDITIONS

SR-SAL-80000217987 C-004  
 07



PROPOSED  
 8" PL-P  
 UNDER WOP# 1312557

PROPOSED  
 8" PL.P.  
 UNDER WOP# 1195876

SCADA TO BE FIELD  
 LOCATED BASED ON  
 I&R REVIEW AND  
 ELECTRICAL PERMIT

SEE DETAIL A  
 (SHEET 06)

SEE DETAIL C  
 (SHEET 06)

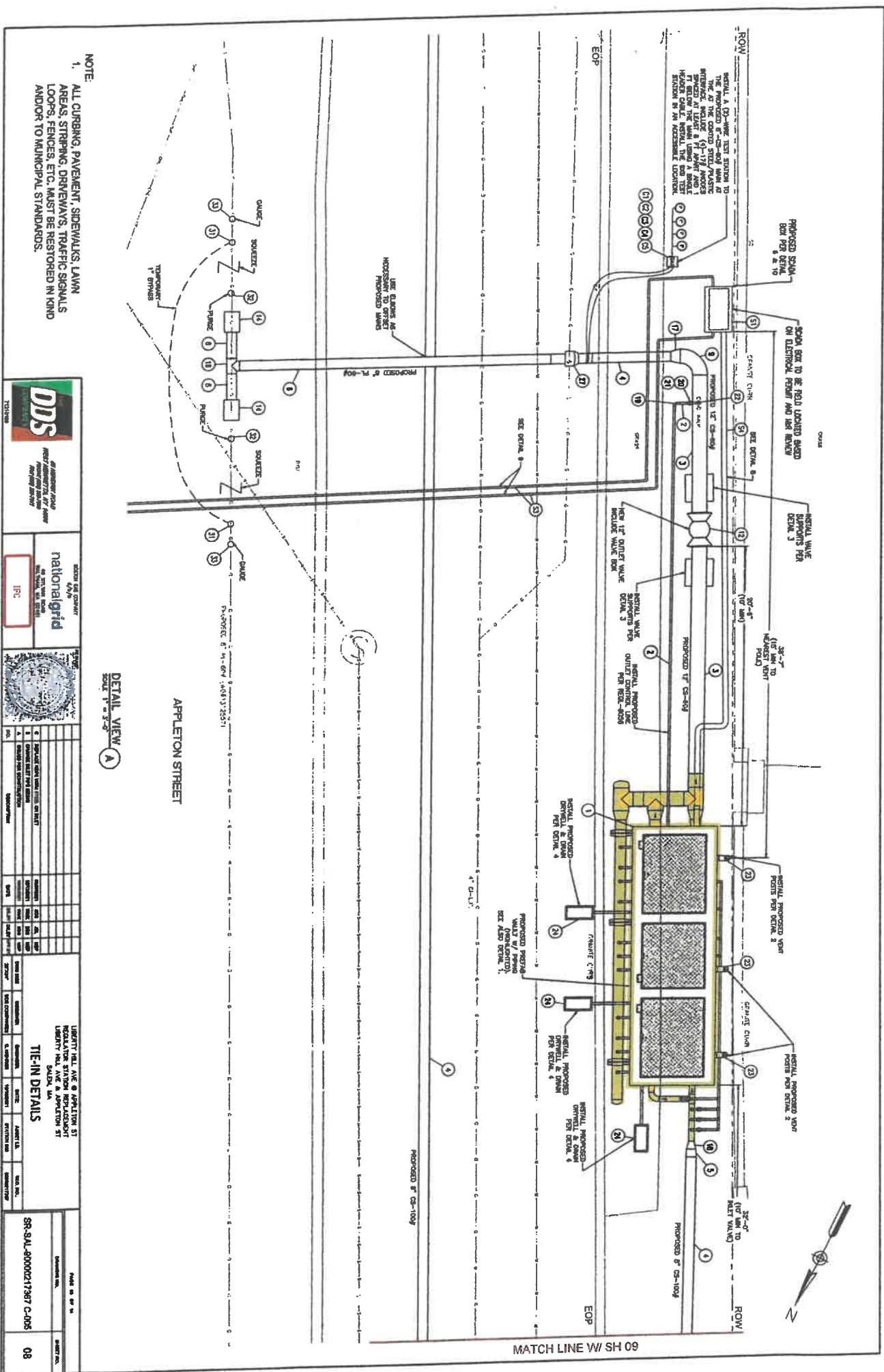
SEE DETAIL B  
 (SHEET 06)

DATE: 11/01/2010  
 11:27:00 AM  
 USER: JEFFREY B. BROWN  
 PROJECT: SR-SAL-80000217987

DATE: 11/01/2010  
 11:27:00 AM  
 USER: JEFFREY B. BROWN  
 PROJECT: SR-SAL-80000217987







NOTE:  
 1. ALL CURBING, PAVEMENT, SIDEWALKS, LAWN AREAS, STRIPING, DRAINWAYS, TRAFFIC SIGNALS, LOOPS, FENCES, ETC. MUST BE RESTORED IN KIND AND/OR TO MUNICIPAL STANDARDS.

DETAIL VIEW (A)  
 SCALE 1/8" = 1'-0"



DESIGN CONTRACT  
 nationalgrid  
 400 N. W. 10th St.  
 Fort Lauderdale, FL 33304  
 IFC



NO.	DESCRIPTION	DATE	BY	CHKD	APP'D
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LIBERTY HILL AVE @ APPLETON ST  
 LIBERTY HILL AVE & APPLETON ST  
 SAULSBURY, VA

TIE-IN DETAILS

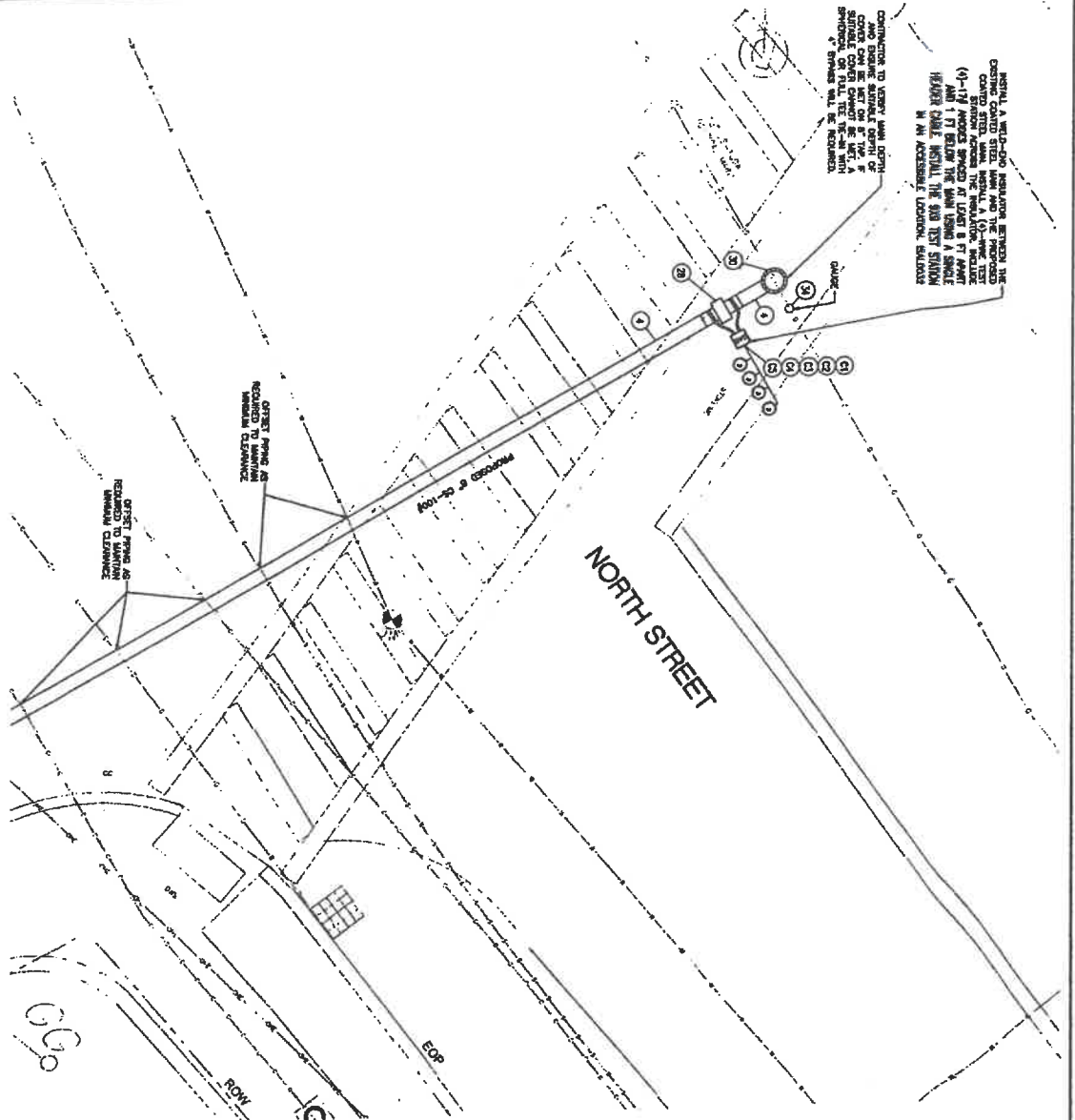
SR-34L-0000217387 C-005  
 08

MATCH LINE W/ SH 09



INSTALL A WEIR-COORD REGULATOR BETWEEN THE  
 CONTROL VALVE AND THE WEIR. THE WEIR SHALL BE  
 (4)-12" DIAMETER. THE WEIR SHALL BE INSTALLED  
 AND IT SHALL BE BELOW THE MAIN LINE. A  
 HEADER SHALL INSTALL THE END TEST STOP  
 IN AN ACCESSIBLE LOCATION, BEHIND

CONNECTION TO VERIFY LEAK DETECTION  
 COVER SHALL BE SET ON 2" DIA. &  
 STAINLESS STEEL COVER SHALL BE INSTALLED  
 IN AN ACCESSIBLE LOCATION. A  
 "PHASE" SHALL BE REQUIRED.



NOTE:  
 1. ALL CURBING, PAVEMENT, SIDEWALKS, LAWN  
 AREAS, STRIPING, DRIVEWAYS, TRAFFIC SIGNALS  
 LOOPS, FENCES, ETC. MUST BE RESTORED IN KIND  
 AND/OR TO MUNICIPAL STANDARDS.

DETAIL VIEW (B)  
 SCALE 1" = 3'-0"



DESIGNER: **DDS**  
 CONSULTING ENGINEERS  
 1000 W. 10TH ST.  
 SUITE 100  
 DENVER, CO 80202

CLIENT: **WATER & SEWER**  
 1000 W. 10TH ST.  
 SUITE 100  
 DENVER, CO 80202

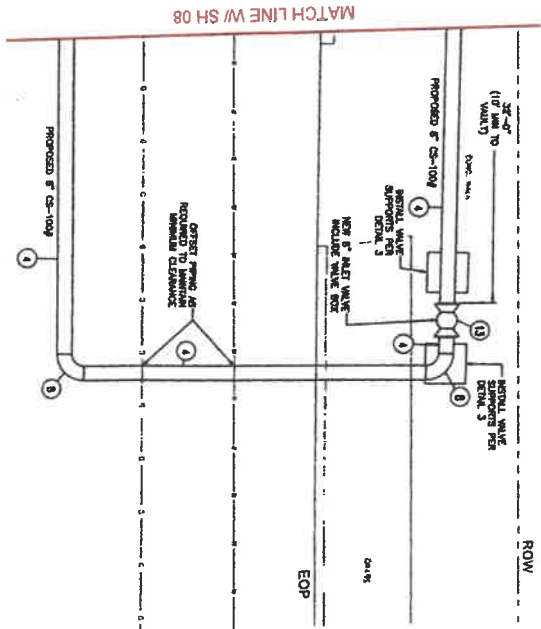


NO.	DESCRIPTION	DATE	BY	CHECKED	DATE	BY	CHECKED
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LIBERTY HALL AVE @ AMPLITION ST  
 REGULATORY STATION REPLACEMENT  
 LIBERTY HALL AVE & AMPLITION ST  
 SHELBY, VA

**INLET TIE-IN DETAILS**

DATE: 08/08/2023  
 DRAWING NO.: SR-SAL-80000217891 C-006  
 SHEET NO.: 09



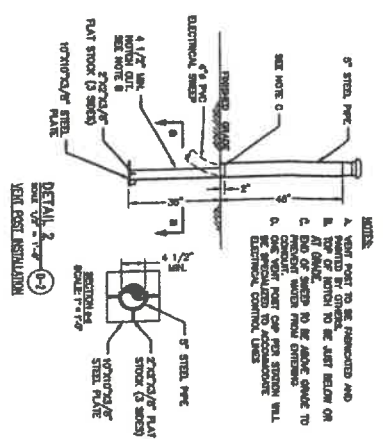
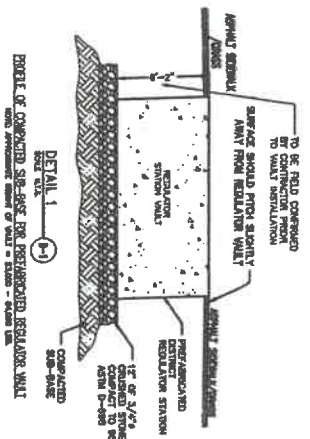
DETAIL VIEW (C)  
 SCALE 1" = 3'-0"



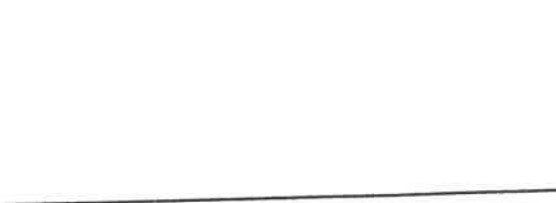
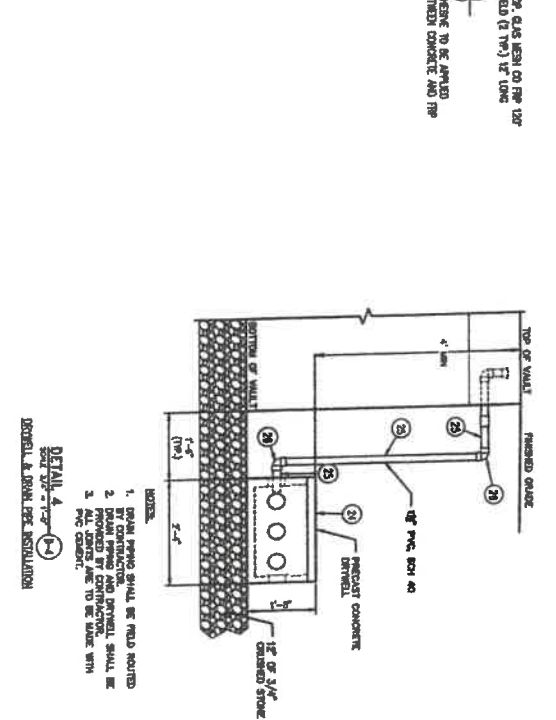
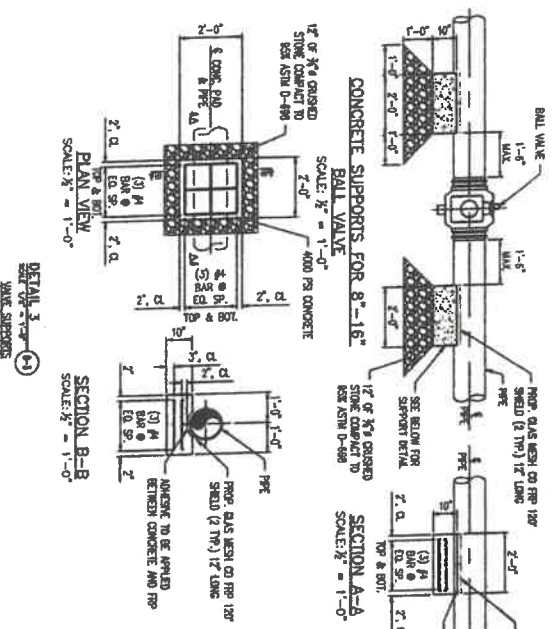








- CONCRETE NOTES:**
1. ALL CONCRETE SHOULD BE REINFORCED WITH #4 BARS AT 12\"/>
  - 2. CONCRETE SHALL BE CURED IN ACCORDANCE WITH THE RECOMMENDED CURING PROCEDURES. CURING PROCEDURES SHALL BE PROVIDED AS REQUIRED IN ACCORDANCE WITH ACI 308.1.
  - 3. ALL CONCRETE SHALL BE REINFORCED WITH #4 BARS AT 12\"/>
  - 4. REBAR OR SUPPORTS SHALL BE CONCREDED.
  - 5. REBAR OR SUPPORTS SHALL BE CONCREDED WITH A MINIMUM OF 1\"/>
  - 6. ALL REBAR AND SUPPORTS SHALL BE CONCREDED WITH A MINIMUM OF 1\"/>
  - 7. ALL CONCRETE SHALL BE REINFORCED WITH #4 BARS AT 12\"/>
  - 8. ALL CONCRETE SHALL BE REINFORCED WITH #4 BARS AT 12\"/>
  - 9. ALL CONCRETE SHALL BE REINFORCED WITH #4 BARS AT 12\"/>
  - 10. CONCRETE REBAR SHALL BE FIELD REINFORCED WITH #4 BARS AT 12\"/>
  - 11. IF CONCRETE IS PLACED IN FIELD, ONE PERCENT SHALL BE REINFORCED WITH #4 BARS AT 12\"/>



nationalgrid  
AFLAK  
IFC



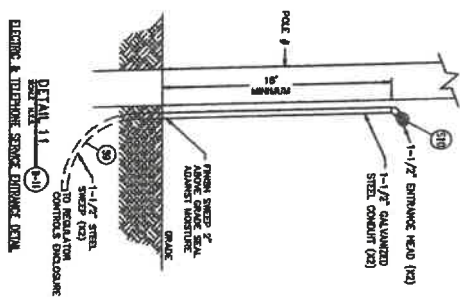
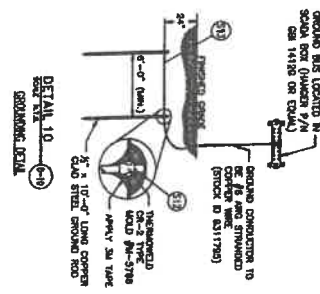
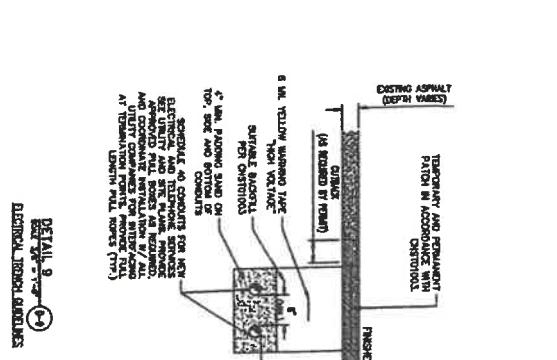
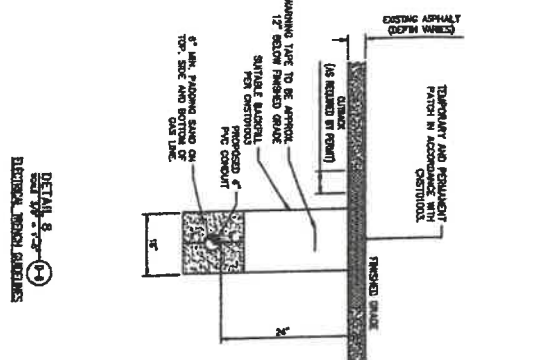
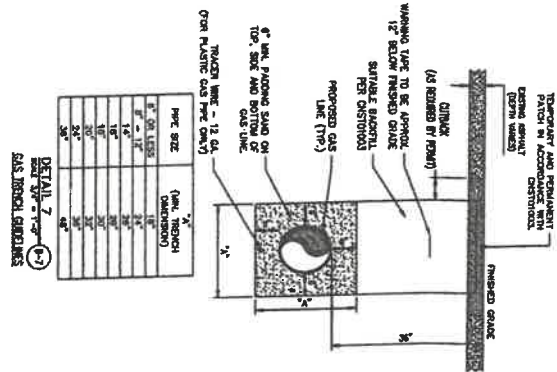
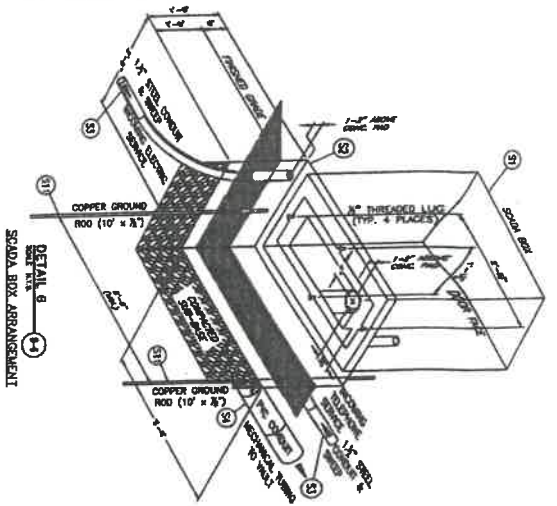
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LIBERTY HILL AEC & INSPECTION ST.  
LIBERTY HILL AEC & INSPECTION ST.  
SALUDA, VA

DETAILS

DATE: 07/11/14  
DRAWN BY: J. B. BROWN  
CHECKED BY: J. B. BROWN  
APPROVED BY: J. B. BROWN

12



NO.	DESCRIPTION	DATE	BY	CHECKED	APPROVED
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LIBERTY HALL AVE @ APPLANT ST  
REGULATOR STATION REPLACEMENT  
SAULSBURY, VA  
DETAILS

SR-SAL-00000217987 M-002

Page 13 of 14

13





# **nationalGrid**

April 8, 2022

Salem City Clerks Office  
93 Washington St,  
Salem, MA 01970

To whom it may concern:

Enclosed please find a petition of National Grid covering the installation of a gas pressure regulator station.

If you have any questions regarding this permit please contact:

Dana Wolkiewicz,  
44 River St,  
Beverly, MA, 01925

Dana.Wolkiewicz@nationalgrid.com  
Phone: 781-697-4631

If this petition meets with your approval, please return an executed copy to:

National Grid Contact:

Dana Wolkiewicz,  
44 River St,  
Beverly, MA, 01925

Dana.Wolkiewicz@nationalgrid.com  
Phone: 781-697-4631

Very truly yours,

Dana Wolkiewicz  
Sr Supervisor Gas Construction

National Grid Contact  
Dana Wolkiewicz, Senior Supervisor Gas Construction  
[Dana.Wolkiewicz@nationalgrid.com](mailto:Dana.Wolkiewicz@nationalgrid.com)  
781-697-4631

Petition of:

National Grid  
44 River St,  
Beverly, MA,  
01915

For the request to reconstruct our gas pressure regulator station and add a SCADA (Supervisory Control and Data Acquisition) System, with an electric service conduit in the public right of way. The installation is located in the shoulder of the roadway on **Appleton St**, adjacent to **#162 North St** and the intersection of **Appleton St & Orne St**.

TO: Lowell City Clerks Office.

Respectfully represents the NATIONAL GRID of 44 River St, Beverly, Massachusetts, that it desires to replace our existing gas pressure regulator station in the shoulder of the roadway on *Appleton St* at the intersection of *Appleton St* and *Orne St*. The proposed installation will replace the existing regulator station at the intersection of *Liberty Hill Ave* and *Appleton St* with a new station meeting National Grid's latest safety standards. This installation includes a new SCADA system, with an electric service conduit, and the necessary sustaining and protecting fixtures, under and across the public way hereinafter named.

Wherefore it prays that after due notice and hearing as provided by law, it be granted permission to excavate the public right of way to run and maintain a gas pressure regulator station, together with such sustaining and protecting fixtures as it may find necessary for the safe running operation of our underground gas structure and SCADA box, said gas pressure regulator station to be located substantially in accordance with the attached plan named – "4" Regulator Station Replacement 100 PSIG to 60 PSIG Liberty Hill Ave @ Appleton St."

NATIONAL GRID

*Dana Wolkiewicz*

BY \_\_\_\_\_  
Sr Supervisor Gas

April 8, 2022



## CITY OF SALEM, MASSACHUSETTS

Kimberley Driscoll  
Mayor

Office of the Mayor

April 14, 2022

Honorable Salem City Council  
Salem City Hall  
Salem, Massachusetts 01970

Ladies and Gentlemen of the City Council:

I am pleased to appoint Veronica Faustino of 64 Rainbow Terrace to the Salem Housing Authority for a five year term to expire April 14, 2027. Ms. Faustino comes with the recommendation of the Salem Housing Authority Tenants Association will fill the seat intended for a tenant representative.

Ms. Faustino is currently the direct service coordinator for the Salem Public Schools' Hawthorne Program, which provides transitional supports for students who have been hospitalized or out of school for medical reasons, as they reintegrate with the district and their classrooms. Prior to that she was an EMT with People Science Inc. Ms. Faustino received her Bachelor's degree from UMass-Boston and completed graduate coursework in mental health counseling and behavioral medicine at Boston University. She serves on the Board of Directors of Family Promise North Shore Boston and chairs the Board's Dismantling Anti-Blackness Committee.

I strongly recommend confirmation of Ms. Faustino's appointment. We are fortunate that she is willing to serve our community in this important role and lend her perspective and dedication to the Authority and its work.

Sincerely,

Kimberley Driscoll  
Mayor  
City of Salem





## CITY OF SALEM, MASSACHUSETTS

Kimberley Driscoll  
Mayor

Office of the Mayor

April 14, 2022

Honorable Salem City Council  
Salem City Hall  
Salem, Massachusetts 01970

Ladies and Gentlemen of the City Council:

I am pleased to appoint Mr. Johnel Roberts of 135 Lafayette Street #212 to the Parks and Recreation Commission for a five-year term to expire October 18, 2023. Mr. Roberts will fill the seat previously occupied by Mr. Emiluis Perez, who has stepped down from the Commission as he is moving. Please join me thanking Mr. Perez for his service to the Commission and to our community.

Mr. Roberts currently runs the Salem-based Young Soldiers Success Group, a nonprofit organization he founded in 2019 to provide mentoring and athletics programs to youth in the community. A first-generation immigrant from the Dominican Republic, Mr. Roberts has been a deeply engaged and positive contributor to the fabric of our city. He has worked as a staffer at the Boys and Girls Club of Greater Salem's summer camp program, coached 6<sup>th</sup> and 7<sup>th</sup> graders on the Salem travel basketball team in the Cape Ann League, and, while a student at Stonehill College pursuing his degree in business administration, volunteered as a coach for the Sharon Shooting Stars basketball league for youth with autism and was a Reading Buddy in the Easton elementary schools.

I strongly recommend confirmation of Mr. Roberts to the Parks and Recreation Commission. We are fortunate that he is willing to serve our community in this capacity and lend his dedication and passion to the Commission and its work.

Sincerely,

Kimberley Driscoll  
Mayor  
City of Salem



**CITY OF SALEM, MASSACHUSETTS**

Kimberley Driscoll  
Mayor

Office of the Mayor

April 28, 2022

Honorable Salem City Council  
Salem City Hall  
Salem, Massachusetts 01970

Ladies and Gentlemen of the City Council:

I reappoint, subject to City Council confirmation, Joy Livramento-Bryant of 22 Puritan Road to the Community Preservation Committee for a term of 2 years to expire May 10, 2024.

I recommend confirmation of this reappointment and ask that you join me in thanking Ms. Livramento-Bryant for her continued dedicated service and commitment to our community.

Sincerely,

Kimberley Driscoll  
Mayor  
City of Salem



**CITY OF SALEM, MASSACHUSETTS**

Kimberley Driscoll  
Mayor

Office of the Mayor

April 28, 2022

Honorable Salem City Council  
Salem City Hall  
Salem, Massachusetts 01970

Ladies and Gentlemen of the City Council:

I reappoint, subject to City Council confirmation, Paul Viccica of 35 Broad Street to the Zoning Board of Appeals for a term of 3 years to expire May 1, 2025.

I recommend confirmation of this reappointment and ask that you join me in thanking Mr. Viccica for his continued dedicated service and commitment to our community.

Sincerely,

Kimberley Driscoll  
Mayor  
City of Salem





**CITY OF SALEM, MASSACHUSETTS**  
Kimberley Driscoll  
Mayor

April 28, 2022

To the City Council  
City Hall  
Salem, Massachusetts

Ladies and Gentlemen of the Council:

I re-appoint Brian Davis of 21 Oakhurst Avenue, Ipswich, MA 01938 to serve as a Constable in the City of Salem for a term to expire February 23, 2025.

Very truly yours,

A handwritten signature in black ink that reads "Kimberley Driscoll".

Kimberley Driscoll  
Mayor



# CITY OF SALEM

In City Council,  
April 28, 2022

**Ordered:**

That the sum of Three Hundred and Fifty Thousand Dollars (\$350,000.00) is hereby appropriated from General Fund Balance Reserved for Free Cash (1-3245) to the following special revenue funds. This transfer is requested in accordance with the recommendation of Her Honor the Mayor.

Fund Name/Description	Amount
Retirement Stabilization Fund 8311	\$ 350,000.00



CITY OF SALEM, MASSACHUSETTS  
Kimberley Driscoll  
Mayor

April 28, 2022

To the City Council  
City Hall  
Salem, Massachusetts

Ladies and Gentlemen of the Council:

Enclosed herewith is an order for Three Hundred and Fifty Thousand Dollars (\$350,000.00) from the General Fund Balance Reserved for Free Cash account (1-3245) to the following Special Revenue funds.

<u>Fund Name/Description</u>	<u>Amount</u>
Retirement Stabilization Fund 8311	\$350,000.00

This request is necessary to afford potential retirements at the end of FY2022 prior to the FY 2023 budget and subsequent appropriation to the Retirement Stabilization Fund being available.

I recommend passage of the accompanying Order.

Very truly yours,

A handwritten signature in black ink that reads "Kimberley Driscoll".

Kimberley Driscoll  
Mayor





**City of Salem, Massachusetts**

Finance Department  
93 Washington Street  
Salem, MA 01970  
[www.salem.com](http://www.salem.com)

**Kimberley L. Driscoll, Mayor**

**Anna Freedman, Finance Director**

---

April 20, 2022

Honorable Salem Mayor Kimberley Driscoll  
Salem City Hall  
Salem, Massachusetts

Dear Honorable Mayor Driscoll:

Enclosed herewith is a request to transfer \$350,000.00 from certified Free Cash to replenish the Retirement Stabilization Fund (8311). This transfer is necessary to afford potential retirements at the end of FY2022 prior to the FY2023 budget and subsequent appropriation to the Retirement Stabilization Fund being available.

After the requested transfer enclosed herewith, the Free Cash balance remaining would be \$6,330,685.53.

Please let me know if you would like to discuss further or require any additional information.

Sincerely,

A handwritten signature in black ink, appearing to read "Anna Freedman".

Anna Freedman  
Finance Director



# CITY OF SALEM

In City Council,

**Ordered:**

April 28, 2022

That the sum of Sixty-Six Thousand, Eight-Hundred Ninety-Eight Dollars and Twelve Cents (\$66,898.12) is hereby appropriated in the "Retirement Stabilization Fund-Vacation/Sick Leave Buyback" account (83113-5146) to be expended for the sick leave buybacks to be paid out for active employees pursuant to collective bargaining agreements with various unions in accordance with the recommendation of Her Honor the Mayor.



**CITY OF SALEM, MASSACHUSETTS**

**Kimberley Driscoll  
Mayor**

April 28, 2022

To the City Council  
City Hall  
Salem, Massachusetts

Ladies and Gentlemen of the Council:

Enclosed herewith is a request for an appropriation of Sixty-Six Thousand, Eight-Hundred Ninety-Eight Dollars and Twelve Cents (\$66,898.12) appropriated from the "Retirement Stabilization Fund-Vacation/Sick Leave Buyback" account (83113-5146).

This transfer is to be expended for the sick leave buybacks to be paid out for active employees pursuant to collective bargaining agreements with various unions.

I recommend passage of the accompanying Order.

Very truly yours,

A handwritten signature in black ink that reads "Kimberley Driscoll". The signature is written in a cursive, flowing style.

Kimberley Driscoll  
Mayor



**KIMBERLEY L. DRISCOLL  
MAYOR**

**LISA B. CAMMARATA  
DIRECTOR OF HUMAN  
RESOURCES**

## **CITY OF SALEM MASSACHUSETTS**

**HUMAN RESOURCES  
98 WASHINGTON STREET, 3RD FLOOR  
TEL. 978-619-5630**

April 25, 2022

Kimberley Driscoll, Mayor  
City of Salem  
93 Washington Street  
Salem, Massachusetts 01970

**RE: Retirement Stabilization**

Dear Mayor Driscoll:

I am respectfully requesting the amount of \$66,898.12 be appropriated in the Retirement Stabilization Fund for sick leave buybacks submitted in accordance with collective bargaining agreements with our various unions.

Sick leave buy-back to be paid out under this appropriation request are for active employees, not retirees, who wish to take advantage of contractual language allowing members to buyback certain amounts of unused sick leave, on an annual basis. A lump sum request for this purpose will allow us to process those requests.

Thank you for your attention and cooperation with this request, and if you have any questions, please do not hesitate to contact me.

Very truly yours,

  
Lisa B. Cammarata

cc: File





# CITY OF SALEM

In City Council,

April 28, 2022

**Ordered:**

To accept the donation from Robert Lutts in the amount of One Thousand Dollars (\$1,000.00) for the upcoming Friends of Park and Recreation Golf Tournament at Olde Salem Greens. These funds will be deposited into the Parks and Recreation Donation Account #24061-4830 in accordance with the recommendation of Her Honor the Mayor.



**CITY OF SALEM, MASSACHUSETTS**  
Kimberley Driscoll  
Mayor

April 28, 2022

Honorable Salem City Council  
Salem City Hall  
Salem, Massachusetts 01970

Ladies and Gentlemen of the City Council:

Enclosed herewith is a request to accept the following donation from Robert Lutts in the amount of One Thousand Dollars (\$1,000.00) to be deposited into the Parks and Recreation Golf Tournament account at Olde Salem Greens.

In order to accept these two donations approval must be given by both the Mayor and City Council. I ask that the members of the City Council join with me in accepting these funds to be used towards the Parks and Recreation Donation Account #24061-4830.

Very truly yours,

A handwritten signature in black ink that reads "Kimberley Driscoll".

Kimberley Driscoll  
Mayor



**CITY OF SALEM, MASSACHUSETTS**  
**PARK, RECREATION & COMMUNITY SERVICES**  
401 Bridge Street, Salem MA 01970  
Tel. (978) 744-0180/(978) 744-0924

Kimberley Driscoll  
MAYOR

April 13, 2022

The Honorable Kimberley L. Driscoll  
Mayor, City of Salem  
93 Washington Street  
Salem, Massachusetts 01970

RE: Friends of Park and Recreation Tourney Sponsor

Dear Mayor Driscoll:

Please accept a check for \$1,000 (ck #1207) donation from Robert Lutts to be deposited into the Park and Recreation Donation Account (Income account # 24061- 4830).

This generous donation is for sponsorship of the upcoming Friends of Park and Recreation Golf Tournament at Olde Salem Greens

Sincerely,

A handwritten signature in black ink, appearing to read "Patricia O'Brien".

Patricia O'Brien  
Superintendent  
Jean A. Levesque Community Life Center  
(Park, Recreation and Community Services)

cc: Dominick Pangallo, Anna Freedman



# CITY OF SALEM

In City Council, April 28, 2022

Ordered:

## **RESOLUTION**

A RESOLUTION THAT THE CITY OF SALEM REQUEST THAT THE MASSACHUSETTS BAY TRANSPORTATION AUTHORITY INCLUDE FUNDING FOR THE ELECTRIFICATION OF THE NEWBURYPORT-ROCKPORT COMMUTER RAIL LINE IN ITS FISCAL YEAR 2023 – 2027 CAPITAL INVESTMENT PLAN (CIP).

Whereas: There is enormous potential along the Newburyport-Rockport MBTA Commuter Rail line to create greater connectivity between access to housing and better paying jobs for residents in the 15 communities where stations are located and the surrounding municipalities; and

Whereas: In November 2019 the MBTA Fiscal Management and Control Board unanimously adopted resolutions to commit the MBTA to an electric Commuter Rail system at the same fare level and frequency as rapid transit which included prioritizing the Environmental Justice Corridor of the Newburyport-Rockport line; and

Whereas: Electric trains are up to 25 times more reliable than our current diesel fleet, which translates to dependable, reliable service and lower maintenance costs; and

Whereas: Electrification of the Newburyport-Rockport line would allow the state of Massachusetts to meet emissions reduction goals as outlined in the Next Generation Roadmap legislation signed into law in March 2021; and



Whereas: The City of Salem has long missed out on rapid transit and is in dire need of affordable, reliable public transportation; and

Whereas: The way to solve the Commuter Rail's existential crisis is to embrace the vision of a 21st century regional rail system that will make it more relevant for residents and riders to utilize while making their trips.

## **NOW, THEREFORE, BE IT HEREBY RESOLVED**

1. That the city council of Salem formally request that the MBTA Board of Directors include funding for the following projects in their Fiscal Year 2023 – 2027 Capital Investment Plan (CIP):
  - a. the electrification of the of the Newburyport-Rockport Line;
  - b. the design and construction of high train platforms for step-free access onto commuter rail trains;
  - c. the construction of infill stations in Everett, Revere, and Salem;
  - d. bus rapid transit from downtown Peabody to Salem Depot to integrate commuter rail ridership



## **CITY OF SALEM, MASSACHUSETTS**

Kimberley Driscoll  
Mayor

Office of the Mayor

April 14, 2022

Honorable Salem City Council  
Salem City Hall  
Salem, Massachusetts 01970

Ladies and Gentlemen of the City Council:

The enclosed resolution endorses the electrification of the Commuter Rail in the MBTA's upcoming five-year capital improvement plan. Salem is part of a coalition of communities served by the Newburyport-Rockport line united in our determination to see progress towards electrification on the Environmental Justice (EJ) Corridor.

Multiple studies have shown the importance of electrifying the entire Commuter Rail. In 2019 the Fiscal Management and Control Board of the MBTA committed to an electric Commuter Rail system. This included prioritizing the EJ Corridor of the Newburyport-Rockport line, the Fairmount line, and the Providence-Stoughton line as part of Phase 1.

Despite these commitments, the MBTA has made limited progress on this initiative. We must meet the urgency of the moment and make the necessary infrastructure investments for Phase 1 on an accelerated timeline now. Without these investments, we risk missing opportunities for once-in-a-generation federal infrastructure funding, in addition to falling short on our emissions reduction goals, outlined in the Next Generation Roadmap signed into law in March 2021.

Our communities cannot afford to wait any longer. The EJ Corridor Coalition is specifically recommending investments be made in the following categories:

### **(1) Electrification of the Newburyport-Rockport Line**

Electric trains are up to 25 times more reliable than our current diesel fleet, which translates to dependable, reliable service and lower maintenance costs. The next step in prioritizing investments in an electrified Commuter Rail include power distribution and catenary installation, modifications to appropriate maintenance facilities, and purchasing a new electric fleet.

Salem City Hall • 93 Washington Street • Salem, Massachusetts 01970  
p: (978) 619-5600 • f: (978) 744-9327 • [www.salem.com/mayor](http://www.salem.com/mayor)

## **(2) High platforms for step-free access and to speed up boarding for everyone**

Step-free access is a critical component of universal accessibility. This gives every rider with reduced mobility easier access to the train. It also allows for faster boarding times, which will save about a minute per stop during peak ridership times. We must begin the process of designing full high-platforms for the line as soon as possible.

## **(3) Infill stations**

Infill stations, including the proposed South Salem platform, coupled with faster service and fare integration, will better connect our Gateway Cities and environmental justice communities to create a more equitable service. Infill stations in Everett, Revere, and Salem will increase connectivity to housing and jobs. Even with new stops, electrification will result in faster travel times and more reliable service that will make the line more attractive.

## **(4) Integration of Peabody ridership through bus rapid transit**

We hope that faster, more reliable service will increase ridership throughout the entire line. A public transit link between downtown Peabody and Salem Depot will increase access to the Commuter Rail and boost ridership. This connection will allow more residents of Peabody to rely on the Commuter Rail as a primary mode of transportation, reducing passenger vehicle commuting throughout the region.

With an increased focus on transit-oriented development it is past time to make these investments. Electrification of the rail will take cars off the road, greatly reduce carbon emissions, improve connectivity for low-income communities, and create easier access to jobs. The Commuter Rail must embrace the vision of a 21<sup>st</sup> century regional rail system that will make it more relevant for residents and riders to utilize while making their trips. We need to invest in this tremendous asset and make it fit for the future.

I recommend adoption of the enclosed Resolution.

Sincerely,



Kimberley Driscoll  
Mayor  
City of Salem

## **CITY OF SALEM**

In City Council, April 28, 2022

**ORDERED:** That the monthly financial reports dated December 2021, January 2022 and February 2022 be received and filed





## CITY OF SALEM

In City Council April 28, 2022

**ORDERED:** That the Committee on Government Services co-posted with the Committee of the Whole meet jointly with the Traffic and Parking Commission and that the Chief of Police or his designee be invited to discuss recent traffic violence and pedestrian safety in Salem.



## CITY OF SALEM

In City Council April 28, 2022

**ORDERED:** That the Committee on Government Services co-posted with the Committee of the Whole meet with the Chief of Police or his designee to discuss the safety issues caused by illegal dirt bikes speeding through Salem's neighborhoods.



## CITY OF SALEM

In City Council April 28, 2022

**ORDERED:** That the Committee on Government Services co-posted with the Committee of the Whole meet to discuss and review the attached draft ordinance relative to Maintenance of City Sidewalks

# City of Salem

*In the year Two Thousand Twenty-two*

**An Ordinance** to amend an ordinance relative to sidewalks.

*Be it ordained by the City Council of the City of Salem, as follows:*

**Section I.** Chapter 38 Streets and Sidewalks, Article III Sidewalk Construction, Division 2 Construction by Owner is hereby amended as follows:

- A) Section 38-165. – Maintenance by City is amended by deleting Section 38-195 in its entirety and inserting:

**“Sec. 38-165. - Maintenance by city.**

- (a) Every sidewalk constructed under this division shall thereafter be kept in repair by the director of public services at the expense of the city.
- (b) Sidewalk materials.
- 1) Brick or concrete surfaces preferred

At the time of installation, construction, maintenance, repair, replacement, or reconstruction of any sidewalk located in the city and open to public travel, whether such sidewalks are located on public or private land, the surface material shall constructed as follows:

- a. Local Historic Districts.

Brick shall be the required surface material for all sidewalks located within the local historic districts.

- b. Downtown B5.

Brick shall be the encouraged surface material for all sidewalks located in the Downtown B5 District, and both brick and concrete shall be permitted therein. All pre-existing brick shall be maintained and replaced in kind.

- c. All other areas.

Concrete shall be the encouraged surface material for all sidewalks located outside of Salem’s historic districts and the downtown B5, and both brick, concrete and asphalt shall be permitted therein. All pre-existing brick shall be maintained and replaced in kind.



2) Existing nonconforming sidewalk surfaces.

Any sidewalk open to public travel whose surface does not comply with this section as of its effective date may remain in such noncompliance until such time as it is maintained, repaired, replaced, and reconstructed, at which time it shall be brought into compliance.

3) Temporary asphalt surfaces.

Notwithstanding anything in this section to the contrary, the city engineer may, in its discretion, permit the temporary use of asphalt surface material for sidewalks:

- a. for a period no than two (2) months, tolled from November 1 through April 30; and/or
- b. for sidewalks abutting undeveloped parcels until issuance of a certificate of occupancy. The department shall specify in writing the time by which brick or concrete surface material, as the case may be, shall replace the temporary asphalt material.”

4) Sidewalk accessibility.

Accessibility is to be prioritized on city sidewalks. A wheelchair ramp must be provided on sidewalks in public ways of the city at pedestrian crosswalks when new or replacement curbing is required at such locations. The department of public services shall give priority to repair sidewalks that contain a wheelchair ramp on sidewalks in the public ways of the city at pedestrian crosswalks.

**B)** Section 38-126 is amended by deleting this section in its entirety.

**Section III.** This Ordinance shall take effect as provided by City Charter.

# Sidewalk Maintenance and Materials Ordinance – FAQ

## **What does this ordinance do?**

This ordinance helps to ensure quality and consistency of our city's sidewalks, improving the accessibility and appearance of our city's streets.

## **Why was this ordinance brought forward?**

Our sidewalks are our most important public spaces, but our city ordinances say very little about them. This clarifies which materials are required or preferred in the downtown B5, local historic districts, and throughout the rest of the city. Revising our sidewalk ordinance will provide clarity in the budgeting process and peace of mind for residents who feel they need to advocate for a preferred material in front of their home. Revisions to the ordinance will ensure that the existing brick sidewalks are maintained in the long term, so our efforts don't need to be duplicated in the future.

## **Which materials would be required or preferred in which parts of the city?**

This ordinance would require concrete or brick within the downtown B5 (with a preference for brick), brick in the Local Historic Districts, the replacement of all pre-existing brick outside of Local Historic Districts, concrete preferred in all other neighborhoods, and (in rare cases) asphalt.

## **Which neighborhood associations and nonprofits support this ordinance?**

Historic Salem Inc, The South Salem Neighborhood Association, North Street Northfields Neighborhood Association, Federal Street Neighborhood Association, Chestnut Street Associates, have provided letters of support for the revised ordinance.

## **What are the environmental implications?**

Brick is a more ecologically friendly paving material than any other commonly used today such as concrete and asphalt. Bricks are made from abundant and sustainable natural resources, they are durable, long-lasting, and reusable. Brick sidewalks laid with traditional sand substrate allow for better groundwater penetration and increase the longevity of our carbon-consuming, oxygen producing, run-off water absorbing street trees.

## **Are brick sidewalks ADA compliant?**

Well-maintained bricks and brick sidewalks offer a wholly ADA compliant paving surface. Passability is rarely an issue of paving type, typically it is an issue of maintenance. Salem's brick sidewalks are some of our oldest, some are nearly a century old and many have not seen meaningful, or regular, maintenance in several decades.

## **Why are sidewalks with pedestrian curb cuts prioritized for maintenance within this ordinance?**

Bricks, which are connected to curb cuts, must be prioritized in the ordinance to ensure safe and accessible passability between sidewalks and streets for pedestrians of all abilities.

### **What is the maintenance plan for brick vs. concrete**

The maintenance plan for sidewalk paving materials in Salem is the same for all finished paving material. General guidelines are followed for the repair or replacement of sidewalks when the following conditions are present; surface deterioration, excessive cracking, raised concrete edges due to tree roots, excessive slopes, or when there are large gaps between joints.

Concerned citizens should report hazardous conditions to the Public Works Department by either 1) submitting a sidewalk repair request to the Department of Public Works or 2) through the SeeClickFix.com which allows photographs to be uploaded for easy reference and the city to prioritize requests based on their location and severity. There is also a 'brick sidewalk repair, replacement, and beautification program' for residents seeking to replace an existing brick sidewalk or extend a neighboring brick sidewalk. The process can take several months and there are no guarantees regarding the quantity and quality of brick available, particularly when only used brick is offered. Lastly, when numerous problematic sidewalk conditions are present and new handicap ramps at intersections and crosswalks are required, the city may use grant funds to bid out the work to be completed by general contractors.

### **What are the cost implications of brick sidewalks?**

While bricks have a higher installation and material cost, they are a much more responsible long-term investment. Over a 40+ year lifespan, maintenance costs can be 85% less than concrete or asphalt which must be replaced periodically in the same time span. Additionally, bricks are often able to be relayed or repurposed in other locations. Every piece of removed asphalt or concrete is disposed of, ending up in a landfill.

### **Will this require concrete sidewalks in our more suburban outlying neighborhoods?**

Though concrete is a preferred material for accessibility and aesthetics outside of the B5 and Local Historic Districts, this still allows for asphalt sidewalks in our suburban outlying neighborhoods, where sidewalks may receive much lighter use.

### **Does this require brick sidewalks in the downtown B5?**

While this ordinance states that brick is the preferred paving material for B5 sidewalks, it allows for both brick and concrete in the downtown B5.

### **What about current brick sidewalks outside of local historic districts?**

This revision codifies a long standing city practice, requiring that current sidewalk materials should be replaced like for like. This ordinance will continue with that unless there is new sidewalk being laid or updated in the downtown B5 and Local Historic Districts in which case, materials should follow the guidance outlined in the new ordinance. Note: Oftentimes, a temporary asphalt patch will be installed to ensure passability of a section of sidewalk or City path which will later be replaced with the appropriate material. This new ordinance includes time-limited use of those temporary patches as well as at undeveloped parcels.



Patricia Morsillo  
President Salem City Council  
93 Washington Street  
Salem, MA 01970

12 April, 2022

Dear President Morsillo and Councilors:

The board of the Chestnut Street Associates supports the motion to repair and install brick sidewalks in historic districts. The brick is part of the fabric of the neighborhood, as it is in many historic areas throughout the country. Indeed, brick is emblematic of historic cities and we should consider this wherever appropriate.

Ideally, this should also include areas abutting districts as well. There are some places where the precise map of the district differs from what we all would consider the same neighborhood, such as when one side of a street is within a district and the opposite side is not. Or when it's borders carve out some properties for no great physical reason. And in appropriate areas throughout the city.

We applaud this effort to maintain the historic character of our city, and strongly urge the use of molded waterstruck brick for its traditional appearance that will blend harmoniously with existing paving, rather than wire-cut brick, which has a modern, machined appearance. The city did that at the new circle to great effect.

We also support using best practices for installation such as those promulgated by the Brick Industry Association. The city will install and maintain the sidewalks, and we are confident they will do the best job possible in a sensitive and maintainable way. The advantages and utility of brick paving is also addressed here and is potentially better than alternatives for many of our tree-lined areas:

<https://www.gobrick.com/docs/default-source/read-research-documents/technicalnotes/14e-accessible-clay-brick-pavements.pdf?sfvrsn=0>

Sincerely,

A handwritten signature in black ink, appearing to read "Andrew Lippman", with a long, sweeping underline that extends to the right.

Andrew Lippman  
President, Chestnut Street Associates  
28 Chestnut Street

To the Council,

We, the board of the Federal Street Neighborhood Association, are writing to support the proposed amendment to the sidewalk ordinance, presuming the error in the draft -- i.e. specifying the deletion and replacement of "Section 38-195" which should be "Section 38-165" -- is corrected.

Maintaining and installing brick sidewalks will serve to enhance the character of our historic neighborhoods and downtown.

Brick is likewise more economic over the long term, as inevitable defects which occur only require the bricks to be reset, as opposed to breaking up, removal and replacement of unsightly and unsuitable asphalt.

The use of brick also advances Salem's goals of being Greener and more resilient to Climate Change. Asphalt is made from petroleum, and its production causes the release of greenhouse gases. Unlike asphalt, a brick sidewalk set in stone dust is pervious, therefore reducing stormwater runoff and preserving the capacity of Salem's storm-water collection system. (For this reason, it is essential that brick sidewalks not be set on an underlying layer of asphalt, as was done in the sidewalk renovation at the Public Library.)

We urge you to support this amendment.

Sincerely,  
FSNA Board



**HISTORIC  
SALEM INC**

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▮ Preservation at work

March 14, 2022

Patricia Morsillo, President  
Salem City Council  
93 Washington Street  
Salem, MA 01970

Dear President Morsillo:

We are writing in support of the proposed amendment to Chapter 38 relative to sidewalks in the City of Salem. Historic Salem, in cooperation with community members throughout Salem, has recently initiated a Brick Coalition which is dedicated to improving the appearance, maintenance, and accessibility of sidewalks throughout the city of Salem, especially in historic areas. The proposed Sidewalk Ordinance Amendment is a pro-active measure to preserve, enhance and maintain brick sidewalks in our historic downtown, as well as to improve sidewalks in all neighborhoods in the city.

Research indicates that environmental, cultural and accessibility goals can all be met through brick paving, with the clear indication that maintenance is at the core of any issue (as is true with all sidewalk materials). We believe that elevating brick as a preferred paving material at a neighborhood scale in historic neighborhoods will help efforts to be environmentally responsive as well as culturally and historically appealing for residents and visitors.

We appreciate that the proposed ordinance highlights Local Historic Districts and the downtown B-5 district as areas appropriate for brick maintenance and installation and identifies concrete and brick as preferable elsewhere. We also note that there are many historic areas in Salem outside of these distinctions including, but not limited to, National Register Districts, such as Bridge Street Neck, and the pre-WW 2 neighborhoods in the urban core, such as Northfields and South Salem. Brick sidewalks in these neighborhoods should be maintained, or new brick sidewalks installed. Asphalt should not be used in these neighborhoods, and we encourage the City to preclude the use of asphalt in these districts either as part of this Ordinance Amendment or at a later date.

Once again, we support this initiative by the City Council to update the sidewalk construction and maintenance ordinance. Thank you for your consideration of these comments.

Sincerely,

Barbara Cleary  
Vice President and Chair, Preservation Committee

9 North Street | Salem, MA 01970  
978.745.0799 | [www.HistoricSalem.org](http://www.HistoricSalem.org)



Dear President Morsillo:

After consideration North St. Northfields Neighborhood Association supports proposed amendment 38 – 165 to Streets and Sidewalks Ordinance in keeping with the historic nature of Salem streets and its prominent position as one of the most historic cities in the east coast. Much of our revenue is derived from tourism and one of the top attractions for visitors to Salem is our historic buildings and the preserved historic appearance of our seaport city. It is wise to preserve our historic assets for the use and enjoyment of our citizens as well as the contribution of cultural tourism to Salem's tax revenue base.

We further ask that the sidewalks be maintained on a regular schedule and that all allowances for accessibility and sidewalk ramps be included in the ordinance and given priority in repair requests.

Yours sincerely,

Anne Sterling & Ellie Rounds Bloom

North Street Northfields Neighborhood Assoc.

Anne Sterling

*"The only thing necessary for the triumph of evil is for good men to do nothing."* Edmund Burke

Co-Chair, North St. Northfields Neighborhood Association  
Friends of Furlong Park Association



April 19, 2022

Salem City Council  
City of Salem  
93 Washington St.  
Salem, MA 01970

Dear Councillors,

The South Salem Neighborhood Association writes in support of the proposed Ordinance Amendment of Chapter 38 in making concrete and brick preferable sidewalk materials and in spotlighting the importance of the diligent maintenance of all of our sidewalks, in the downtown as well as in our residential neighborhoods.

We believe that the historic value and environmental benefit of brick sidewalks needs to be recognized and supported by the Council and the City, with the coinciding priority that they be well maintained as a city standard, especially in historic areas. We do not support asphalt as an appropriate sidewalk material in our downtown or in our neighborhoods and agree that it should be used only as a short-term option until either concrete or brick, the preferred materials, can be installed or appropriately repaired.

Thank you for your consideration of our comments as you take up this important amendment.

Sincerely,

*Mary R. Wilbert*

Mary R. Wilbert  
President  
South Salem Neighborhood Association

/mrw



CHIEF OF POLICE  
LUCAS J. MILLER

# City of Salem, Massachusetts

## Police Department Headquarters

95 Margin Street  
Salem, Massachusetts 01970  
(978) 744-2204

25 March 2022

Ms. Ilene Simons  
City Clerk  
Salem City Hall  
93 Washington Street  
Salem, Massachusetts 01970

Dear City Clerk Simons,

As you are aware, earlier this month the Salem Police Department acquired a community resource dog (CRD), "Derby". The CRD program is relatively new to the law enforcement profession and with Derby being new to the Department, I would like to take the opportunity to introduce Derby to members of the City Council at their next in-person meeting. In addition to the introduction, I will provide the Council with a brief overview of how the CRD program will be implemented in Salem.

I am looking forward to addressing the Council and introducing the newest member of the Department.

If you require any additional information on this request, please feel free to contact either myself or my administrative aide, Mr. Bob Mulligan, at (978) 744-0171 extension 50103 ([rmulligan@salempd.net](mailto:rmulligan@salempd.net)).

Sincerely,

A handwritten signature in black ink that reads "Lucas J. Miller".

Lucas J. Miller  
Chief of Police

cc: Mayor Driscoll  
Captain Burke  
Officer Rondinelli



# CITY OF SALEM PLANNING BOARD

2022 APR 22 AM 10:10

CITY CLERK  
SALEM, MASS

## Report of the Planning Board to City Council Re: Entrance Corridor Overlay District

April 22, 2022

At its meeting on April 14, 2022, the Planning Board discussed a proposed amendment to the Zoning Ordinance to amend Section 8.2 to allow for limited substitution of smaller ornamental trees for larger tree species within the Entrance Corridor Overlay District, subject to Planning Board approval via the creation of a design waiver, as well as other dimensional changes to support healthy planting. This item which was referred from the April 13, 2022, joint public hearing with the City Council.

The Planning Board voted eight (8) in favor (Bill Griset, Tom Furey, Helen Sides, Sarah Tarbet, Todd Waller, Carole Hamilton, Kirt Rieder, Zach Caunter) and zero (0) opposed to recommend that the City Council adopt the proposed amendment to the Zoning Ordinance with the following comment:

- Proposed addition: "Trees are a crucial component of the entry corridor, available in a broad range of species and sizes. Shade trees are preferred for their medium to large canopy width and height for a full range of community benefits identified in the tree ordinance, including cooling and stormwater uptake."

If you have questions regarding this matter, please contact Tom Daniel, AICP, Director of Planning & Community Development, at 978-619-5685.

Sincerely,

William Griset  
Chair

CC: Ilene Simons, City Clerk



# CITY OF SALEM PLANNING BOARD

2022 APR 19 PM 12:16

CITY CLERK  
SALEM, MASS

## Report of the Planning Board to City Council Re: Bridge Street Neck Overlay District

April 15, 2022

At its meeting on April 14, 2022, the Planning Board discussed a proposed amendment to the Zoning Ordinance to amend Section 8, *Special District Regulations*, to add Section 8.7, establishing the Bridge Street Neck Overlay District, which was referred from the April 13, 2022, joint public hearing with the City Council.

The Planning Board voted seven (7) in favor (Bill Griset, Helen Sides, Sarah Tarbet, Todd Waller, Carole Hamilton, Kirt Rieder, Zach Caunter) and one (1) opposed (Tom Furey) to recommend that the City Council adopt the proposed amendment to the Zoning Ordinance, with the following comments:

- Strike administrative review;
- Transition Building Design Standards to guidelines;
  - o If retained as "Standards" any change would require an amendment. This change is meant to create flexibility and the opportunity for updates.
- Planned Unit Development (PUD): The Planning Board does not recommend prohibiting PUDs in the overlay district because it is unnecessarily restrictive to the few parcels that might benefit.
- Density: The Planning Board recommends a lot area per dwelling unit below 1700 square feet to encourage walkability and affordability.
- Design Review Board: The Planning Board feels that the DRB issuing a design recommendation is sufficient.
- Substitute hedging for shrubbery.
- Differentiate between common, open, and vehicular spaces.

If you have questions regarding this matter, please contact Tom Daniel, AICP, Director of Planning & Community Development, at 978-619-5685.

Sincerely,

William Griset  
Chair

CC: Ilene Simons, City Clerk

# *City of Salem*

---

*In the year Two Thousand and Twenty-Two*

**An Ordinance** to amend an ordinance relative to zoning.

**Section 1. Amend Section 8 Special District Regulations** by adding the following new section:

## **8.7 Bridge Street Neck Overlay District**

**8.7.1 Purpose.** The purpose of Bridge Street Neck Overlay District (BSN) is to enhance the Bridge Street Neck neighborhood by helping to achieve the vision and principles of the Bridge Street Neighborhood Vision Update Plan (2020 or most recent edition), including:

1. Maintain a thriving small and local business district.
2. Encouraging clusters of retail and mixed-use development on Bridge Street.
3. Developing a vibrant, family-friendly neighborhood, affordable to residents across a range of incomes.
4. Developing a sense of place based on the neighborhood's location, unique assets, history, and architecture.
5. Enhancing the public realm through improved site-planning, sidewalk amenities, and open spaces.
6. To provide certainty regarding acceptable land uses and development design.

**8.7.2 District Boundaries.** The Bridge Street Neck Overlay District shall be established as an overlay district superimposed on all underlying zoning districts within the Bridge Street Neck Overlay District boundaries. The boundary extends to all parcels north of Webb Street that abut Bridge Street, in addition to 2, 4, 6, 8, and 10 Pierce Avenue; 2, 4, 6, 8 and 10 Waite Street; 38 Rear Bridge Street; 3-5, 6, and 7 Cromwell Street; 18 East Collins Street; and 9 Lathrop Street, as shown on the official zoning map.

## **8.7.3 Eligible Uses.**

1. A developer may elect either to develop a project in accordance with the requirements of the Bridge Street Neck Overlay District, or to develop a project in accordance with requirements of the regulations for use, dimension, Section 8.4, and all other provisions of the Zoning Ordinance governing the underlying zoning district(s).
2. If a use is permitted by the Bridge Street Neck Overlay District but not by the underlying zoning, it shall be considered a permitted use and is not eligible for a special permit pursuant to Section 3.3.2 of the Salem Zoning Ordinance.
3. Uses are permitted as set forth in the following Table of Bridge Street Neck Uses. Uses permitted through a Special Permit are granted by the Planning Board.



<b>BRIDGE STREET NECK TABLE OF USES</b>	
<b>Residential</b>	
Dwelling, Two-Family	Y
Dwelling, Multifamily	SP
Dwelling unit above first floor retail, personal service, or office use	Y
Planned unit development	N
<b>Office</b>	
Business or professional office	Y
Medical or dental office	Y
<b>Retail</b>	
Retail store, except department store	Y
Personal service establishment	Y
Restaurant; snack bar	Y
Restaurant	Y
Bank, financial agency	Y
<b>Industrial</b>	
Brewery, distillery, or winery with tasting room	Y
Computer hardware development	SP
Light manufacturing	SP
Publishing and printing	Y
Research, laboratories, and development facilities	SP
<b>Commercial</b>	
Arts and crafts studios and workshops	Y

**8.7.4 Administrative Plan Review Applicability.** The Director of Planning and Community Development or their designee shall review all Administrative Plan Review applications in accordance with the criteria set forth in 8.7.12, with no testimony or submission of evidence as would be allowed at a public hearing. Each project hereunder shall require an administrative plan review:

1. An addition of less than two thousand (2,000) gross square feet.
2. Façade Improvements.

**8.7.5 Site Plan Review Applicability.** The Planning Board shall review all such submitted plans in accordance with the provisions of Section 9.5 and 8.7. At the time of filing an application under Section 8.7.13, the applicant shall provide a copy of the application to the City of Salem Design Review Board. Each project hereunder shall require site plan review approval:

1. An addition of more than 2,000 square feet; or,
2. Any project that includes a request for a Development Standard Waiver Special Permit.

**8.7.6 Development Standard Waiver Special Permit Applicability.** Recognizing that the Development Standards cannot anticipate all circumstances or innovative approaches, a waiver may be requested from the development standards specified in Section 8.7.8.

to provide the ability to create appropriate variations.

### **8.7.7 Building Design Guidelines**

#### **1. Purpose and overview**

- A. Building investments should strengthen the neighborhood, reinforce its intimate and historic scale, and contribute to the vitality, activity, and continuity of a walkable place. The following design guidelines are intended to help achieve these goals. Although they are advisory in nature, project applicants are strongly encouraged to incorporate, as applicable. Explanatory diagrams are provided to assist with the various guidelines.
- B. Bridge Street Neck has experienced almost continuous development over its three centuries of existence, resulting in a variety of historic architectural styles present. While new development need not mimic previous buildings or styles, it should be designed in a way that complements and harmonizes with the character of nearby existing properties with respect to architecture, scale, landscaping, and screening.
- C. Additions to historic buildings, as defined by having a Form B on the State Massachusetts Cultural Resource Information System (MACRIS) database or listed as a contributing resource in the Bridge Street Neck National Register Historic District, should be clearly subordinate to the historic building.
- D. Historic architectural features should be preserved whenever possible or reproduced as needed. Retention of architectural detailing and inclusion of similar detail in new projects is encouraged.

#### **2. Site and building design**

- A. **Building Orientation.** Buildings should be located and oriented towards the street.
- B. **Building Mass.** The historic context of the neighborhood should be respected. Large buildings should be visually reduced by changes in the massing, architectural elements, and other articulations. Generally, larger buildings should be articulated into bays of 40 feet or less.
- C. **Setbacks.** Historically, buildings had minimal front setbacks along Bridge Street. New development should replicate this streetscape pattern with minimal setbacks, as well. Setbacks are appropriate to provide outdoor seating. Setbacks may also be used strategically to set apart signature buildings.
- D. **Reduce Visual Impact of Parking.** Parking should be located in the rear of buildings. Where possible, driveways should be consolidated.
- E. **Sensitive Building Height.** Where appropriate, building height of proposed development should be sensitive to its context, such as through stepping down a portion of the building to meet the height of adjacent buildings, if the adjacent buildings are historic.
- F. **Interesting Rooflines.** No one roof type characterizes the neighborhood's buildings, although the front gable is most common. Roofs of new buildings should respect the styles that currently exist.

The following two diagrams (Figure 1 and Figure 2) provide two views of a hypothetical development to visually represent the provisions of 8.7.7.2.A through 8.7.7.2.F. Figure 1: Site and building design diagram (1 of 2)

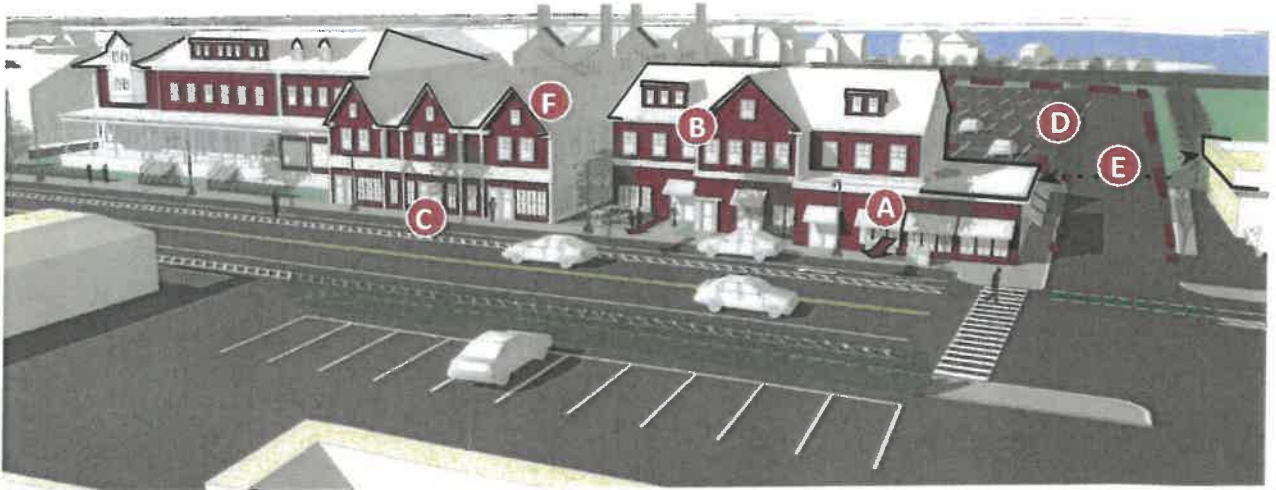
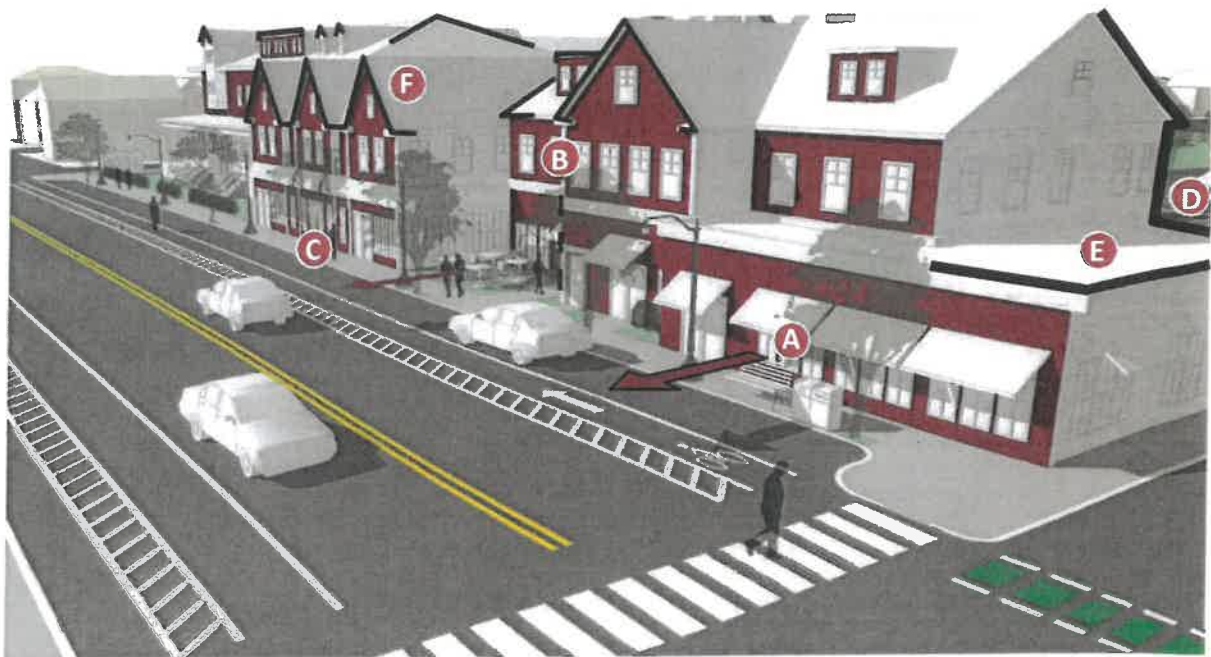


Figure 2: Site and building design diagram (2 of 2)



### 3. Façade Design

- A. **Facade Organization.** Facades should be divided horizontally into a base, middle, and top with distinct visual cues for each section. The base provides the anchor for the structure, while displaying ground-floor activity. The middle portion reflects uses on upper floors, often with smaller, regularly spaced windows. The top of the building transitions and integrates with the roof.
- B. **Active Ground-floor Uses.** Activity on the ground floor, facing the street, strengthens the vibrancy of the neighborhood. This activity traditionally includes shops, restaurants,

and other businesses. It may also include residential lobbies, community rooms, exercise rooms, cafes, or gallery spaces.

- C. **Ground-floor Windows.** The ground floor of buildings should generally include a higher percentage of window glazing compared to the rest of the building (60% is often a target). Glass should be clear, as opposed to tinted or reflective.
- D. **Canopies and Awnings.** Canopies and awnings can be used to provide a human-scaled element to the ground-floor of buildings by reducing the building's appearance of bulk and mass. They also provide protection from the elements.

- E. Architectural Details. Facade details help reinforce the neighborhood’s character. Facade details can draw attention to the doors, windows, ground floor levels, cornices, and eaves of buildings. They can also help modulate the mass of large buildings by dividing them into a pattern of smaller bays.
- F. High Quality Materials. High quality and durable materials, such as clapboard (wood or composite), stone, and brick, should be used where possible. Vinyl siding and other synthetic materials should generally be avoided.

The following two diagrams (Figures 3 through 6) provide diagrams and photos to visually represent the provisions of 8.7.7.3.A through 8.7.7.3.E.

Figure 3: Facade Design Diagram (1 of 4)

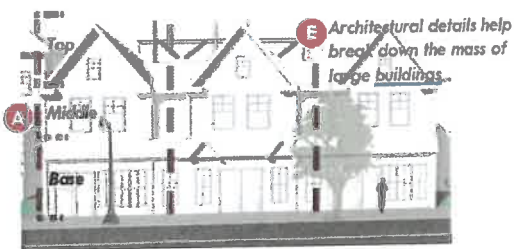


Figure 4: Facade Diagram (2 of 4)

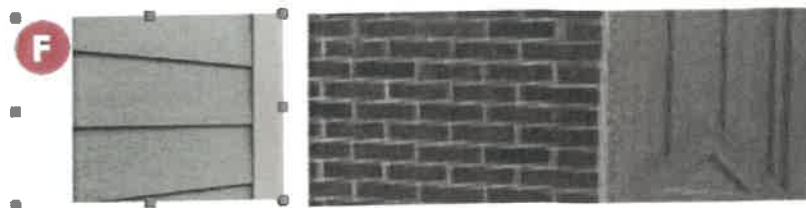


Figure 5: Facade design diagram (3 of 4)



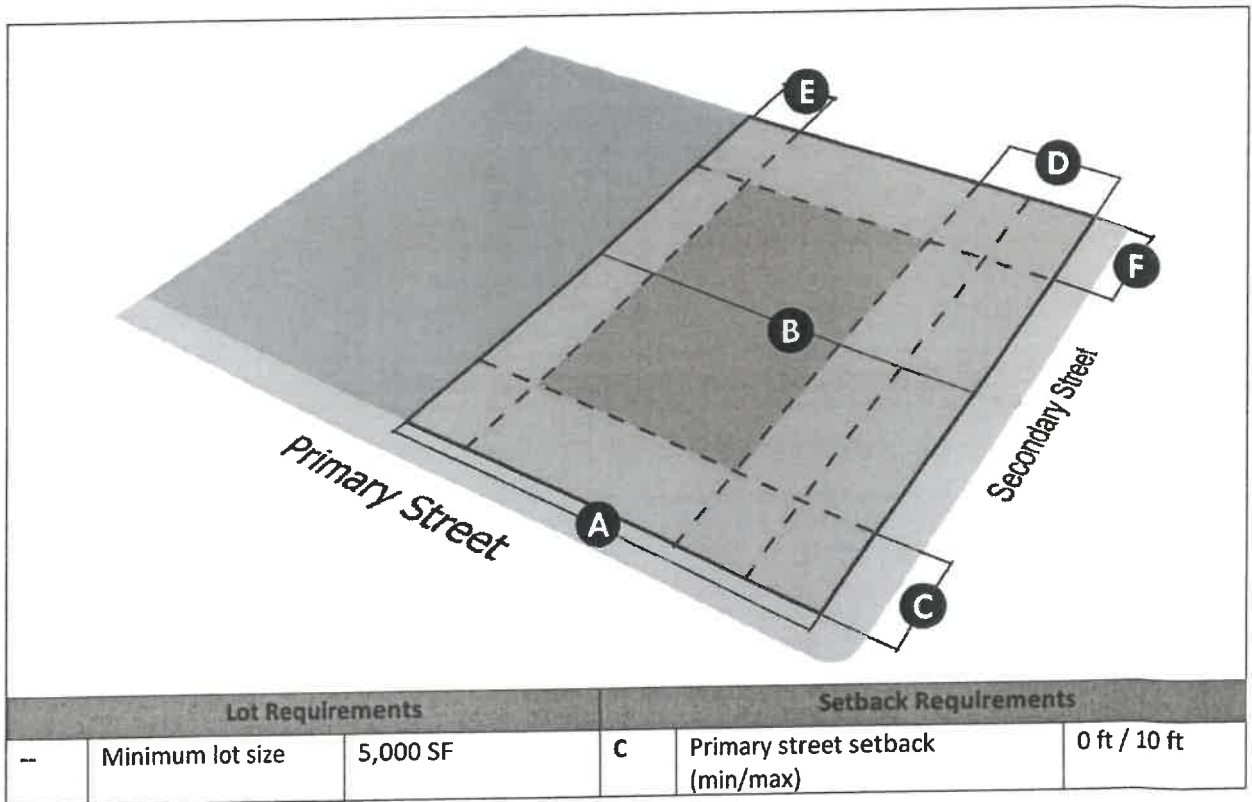


Figure 6: Facade design diagram (4 of 4)



### 8.7.8 Development Standards

1. **Lot Standards.** In order to achieve the purpose of this ordinance, all development shall comply with the following standards:



Lot Requirements			Setback Requirements		
--	Minimum lot size	5,000 SF	C	Primary street setback (min/max)	0 ft / 10 ft
A	Minimum frontage	60 ft	D	Secondary street setback (min/max)	10 ft / 20 ft
B	Minimum lot width	60 ft	E	Side setback (min)	10 ft or 0 ft if a common wall
--	Open space	15% if containing residential; 0% otherwise	F	Rear setback (min)	20 ft

- A. Primary street refers to Bridge Street
- B. Secondary street applies to corner lots and refers to the street which intersects Bridge Street
- C. Where a primary street setback is greater than 0 ft, the following standards shall apply:
  - a. Within the front setback, the space shall be used for one or more combination of:
    - i. Outdoor seating associated with a ground-floor establishment.
    - ii. Publicly available open space, such as a plaza or the like.
    - iii. Stoops leading to the building's entryway. In such cases the area outside of the stairs and associated walkway, if any, shall be highly landscaped with a variety of native plants, shrubs, grasses, and trees.
    - iv. An enclosed extended shopfront occupying a portion of the building. See standards for extended shopfront in Section [insert].
    - v. Front garden space.
  - b. The Planning Board may allow a waiver for greater setbacks whereby in its judgement, doing so will contribute to an improved public realm. Space within the additional front setback shall not be used for parking purposes.
  - c. Parking is prohibited within the front setback, unless a waiver is granted by the Planning Board. Consideration will be given only in situations where locating parking beyond the front setback is considered impractical or infeasible.
  - d. The setback requirements herein shall not be considered the setbacks for trees in Salem Code of Ordinances Section 43-51. The provisions of Chapter 43, public trees remain applicable.
- D. Open space requirements may include common roof-decks, courtyards, and other communal spaces. Private balconies are excluded from meeting the minimum requirement.

## 2. Building Standards

The following table provides standards for new buildings constructed in the BSN.

Building Requirements	
Building height (max)	38 ft for pitched roof building, excluding Mansard 35 ft flat roof building or Mansard

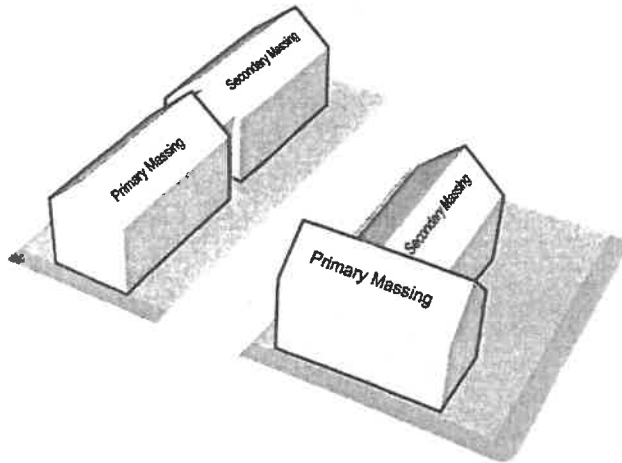
Ground floor height (min)	12 ft commercial; 10 ft otherwise
Second story height (min)	10 ft
Half story height (min)	10 ft
Ground floor glazing (min)	60% commercial 30% other
Maximum number of dwelling units	1,700 sf of lot area per dwelling unit
Space between multiple buildings on the same lot	10 ft or 0 ft if a common wall
Lot coverage (max)	75%
Building length (max)	100 ft
Vertical façade modulation (min)	40 ft

### 3. Building Design Standards

New buildings and substantial building renovations shall be reviewed with reference to the building design standards. If, in the opinion of the Planning Board, deviations from the standards and guidelines contained herein would improve the overall design of the proposed project, the standards in question may be waived.

1. The Planning Board may permit multiple buildings and uses on the same lot, provided all standards are met.
2. A building shall be comprised of a primary mass and an optional secondary mass. The primary massing of a building must be of rectangular form. The primary massing may be oriented either with its narrow end or long side toward the front lot line. The building façade must be built parallel to the front lot line.

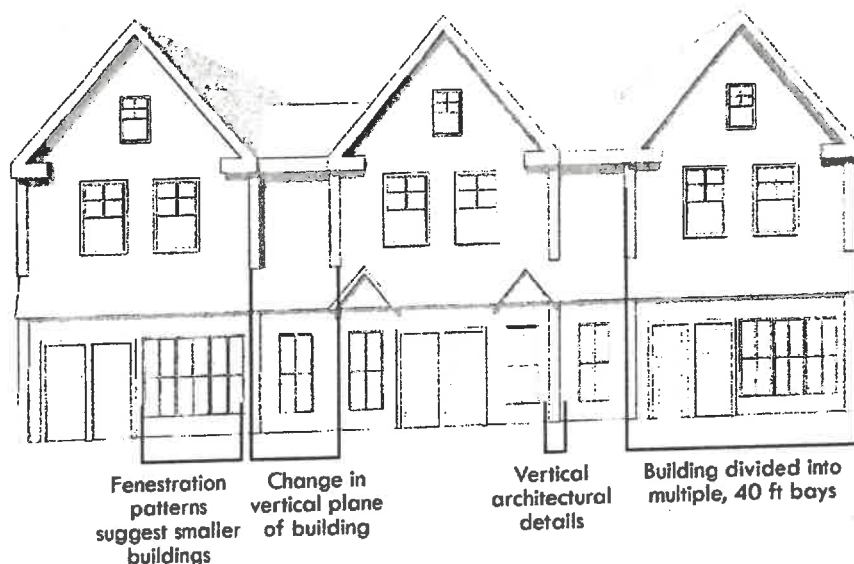
Examples of primary and secondary building mass assembly



3. Buildings shall have roofs that complement the existing context, including front gable, side gable, hip, Mansard, and flat roofs. The Planning Board may permit additional roof styles where it believes the applicant has demonstrated that the proposed roof will improve the visual character along Bridge Street.

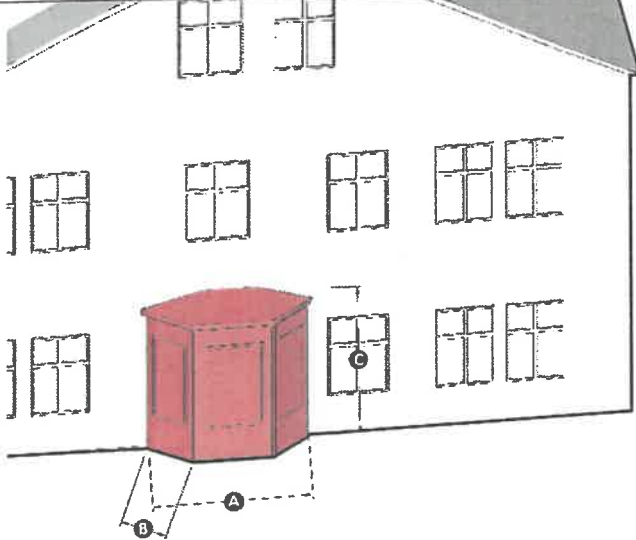
4. Multi-story buildings with ground-floor commercial spaces must have one principal entrance for each commercial space in addition to any principal entrance(s) necessary for upper floors.
5. Unless otherwise noted, façade glazing shall consist of highly transparent, low reflectivity glass. Façade glazing requirements are only applicable to front facades along the primary street. Façade glazing is measured between two feet and twelve feet of the first floor façade.
6. Vertical façade modulation reduces the visual bulk of large buildings by designing buildings to visually read as smaller buildings. To achieve this effect buildings greater than 40 feet in width shall be articulated through changes in the vertical plane of the building (minimum one foot and maximum 4 feet), vertical architectural details such as columns, pilasters, and the like, or fenestration patterns that suggests smaller buildings.

**Example of vertical articulation elements**

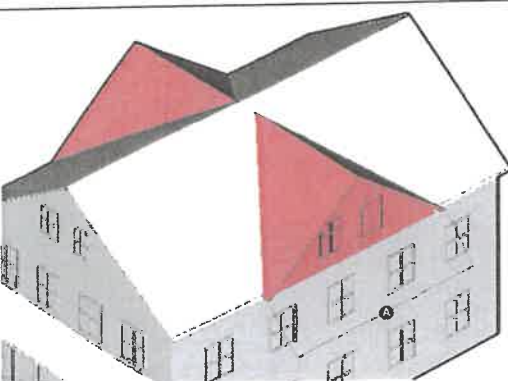


7. Building components are accessory elements integrated into the building's massing that increase a building's usefulness and provide a means for achieving variety and individuality in design.
  - i. The intent of this section is to provide a clear and flexible system for enabling the construction of new buildings and modification of existing buildings in a manner consistent with the neighborhood's character and vision.
  - ii. The following components and associated standards are allowed on buildings with frontage on the primary street.
  - iii. The Planning Board may permit additional building components via special permit if it determines the proposed component further enhances the building's design in relationship to the neighborhood character, is appropriate within the neighborhood context, and is consistent with the Purpose of this Ordinance.

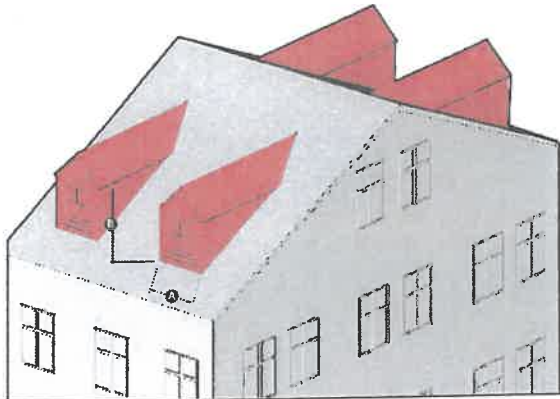
i. Bay window

		<p>Description: an enclosed window assembly extending from the face of a building element to permit increased light, multi-dimensional views, and articulate a building façade.</p>
A	Width (max)	16 ft
B	Projection (min / max)	0.5 ft / 4 ft
C	Top Plate height	Max equal to total number of full building stories
--	Fenestration	30% min
<p>Standards:</p> <ol style="list-style-type: none"> <li>1. Bay windows must attach to a single building face and may not wrap around corners.</li> <li>2. Bay windows may be three-sided or curved.</li> </ol>		

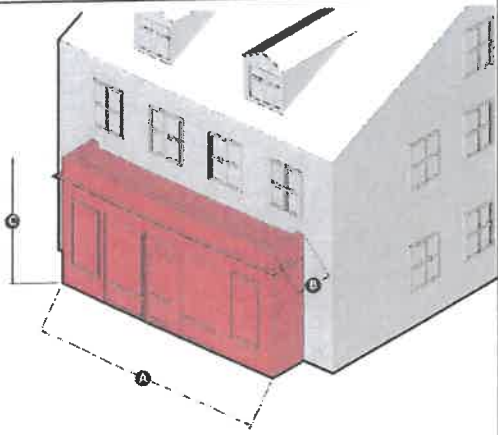
ii. Cross Gable

		<p>Description: a gable roof that projects perpendicular from the roof of the primary building to increase the habitable space within the roof</p>
A	Width	Equal to the width of the narrowest building face
--	Fenestration	20% min
<p>Standards:</p> <ol style="list-style-type: none"> <li>1. Height may not be higher than the ridge beam of the primary building</li> <li>2. Two cross gables of equal size and roof pitch may be used together</li> </ol>		

iii. Dormer

		<p>Description: a window or group of windows with a gable, hip, or shed roof that projects vertically from the roof of a building element, designed to provide increased light and expand the habitable space within a roof.</p>
<b>A</b>	Width (max)	Window(s) width + 18 inches
<b>B</b>	Height (max)	No higher than height of roof
<p>Standards:</p> <ol style="list-style-type: none"> <li>1. Setback from ridgeline must be 3 ft or greater.</li> <li>2. Setback from side wall must be 1 ft or greater.</li> <li>3. Separation between dormer(s) shall be a minimum of 50% of the dormer width.</li> <li>4.</li> </ol>		

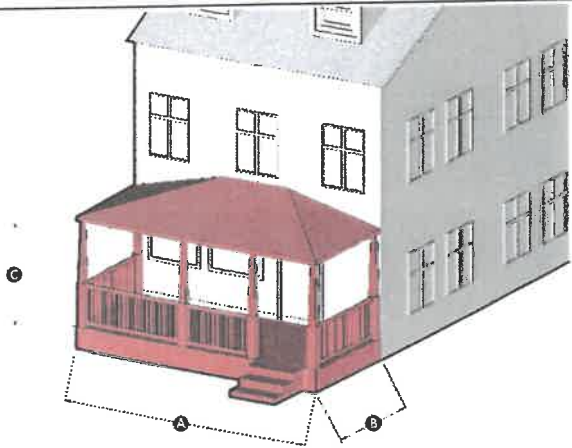
iv. Extended Shopfront

		<p>Description: an extension of the front of a building to provide new or expanded commercial space and a shopfront.</p>
<b>A</b>	Width (max)	No more than the width of the building face to which it attaches
<b>B</b>	Projection (max)	10 ft
<b>C</b>	Height (max)	Equal to the total number of full building stories
--	Fenestration (min)	70% ground floor 30% upper floors
<p>Standards:</p> <ol style="list-style-type: none"> <li>1. Extended shopfronts may wrap around corners of the building face.</li> <li>2. An extended shopfront may have a flat roof.</li> </ol>		

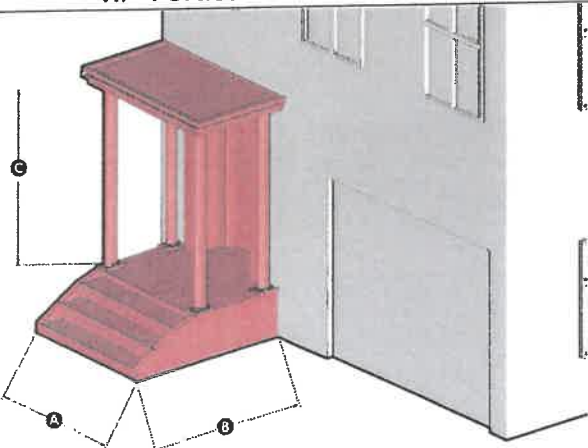


3. Extended shopfronts are permitted only on new construction.

v. Porch

		<p>Description: a raised platform with stairs that provides access to a building entrance.</p>
<b>A</b>	Width (max)	No more than the width of the adjoining building face
<b>B</b>	Projection (min / max)	6 ft / 12 ft
<b>C</b>	Height (min)	7 ft
--	Fenestration (min)	60% when enclosed
<p>Standards:</p> <ol style="list-style-type: none"> <li>1. Porches may be partially or fully enclosed.</li> <li>2. Stairs may extend off the front or side of the porch, except when a porch is located on an upper floor in which case the stairs cannot be located along the primary frontage.</li> <li>3. The space between piles or piers shall be hidden with material or plantings on all sides.</li> <li>4.</li> </ol>		

vi. Portico

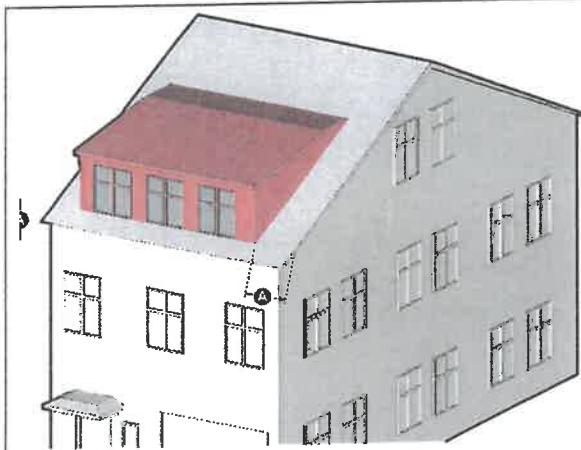
		<p>Description: a roofed structure above a stoop or platform supported on columns or pillars, which shields occupants from inclement weather and provides access to a building entrance.</p>
<b>A</b>	Width (min)	3.5 ft
<b>B</b>	Projection (min / max)	3.5 ft / 6 ft

<b>C</b>	Height (min)	8 ft
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**Standards:**

1. A portico may not exceed the height of the primary ridge beam on the building element to which it attaches.
2. A portico must be elevated on a platform at least 1' from the ground.

**vii. Shed Dormer**



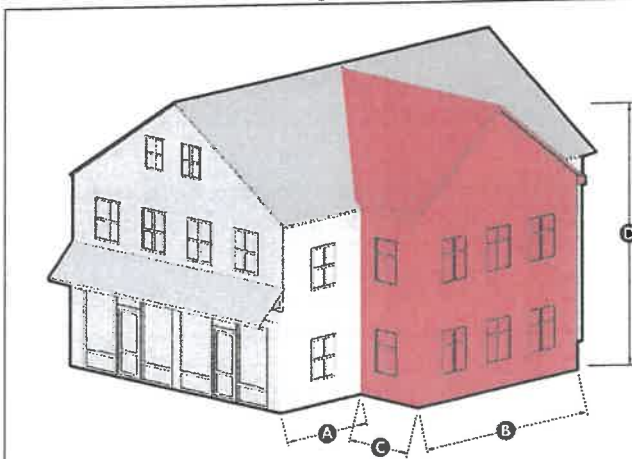
**Description:** a room, or portion of a room with a shed roof that projects vertically from the roof of a building element, designed to provide increased light and expand the habitable space under a roof.

<b>A</b>	Gable end setback (min)	1 ft
--	Fenestration	30% min

**Standards:**

1. Windows should be vertically proportioned and no more than 3 ft wide.
2. Shed dormers must have a minimum 4:12 slope.
3. The dormer must not extend beyond the eave line.

**viii. Side wing**

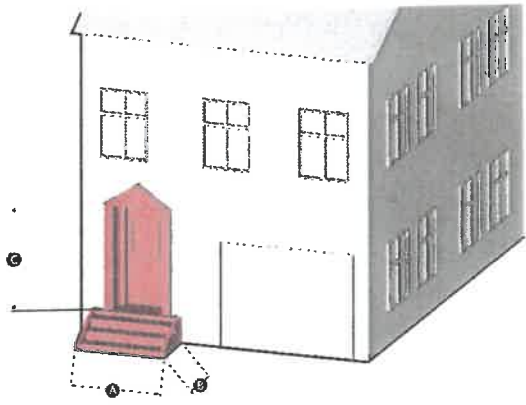


**Description:** a multi-story extension from the side walls of a building's primary or secondary massing.

<b>A</b>	Setback from facade (min)	8 ft
<b>B</b>	Width (max)	2/3 width of primary building
<b>C</b>	Projection (max)	12 ft

<b>D</b>	Height	Equal to or less than the height of the primary building
--	Fenestration (min)	20%
Standards:		
<ol style="list-style-type: none"> <li>1. A primary building mass may have only one side wing per side.</li> <li>2. Additional building components, e.g. doors and porches, may attach to the side wing.</li> </ol>		

ix. Stoop

		Description: a component featuring a set of stairs with a landing to the entrance of a building.
<b>A</b>	Width (min)	3.5 ft
<b>B</b>	Projection (min)	3.5 ft
<b>C</b>	Height (min)	7 ft
Standards:		
<ol style="list-style-type: none"> <li>1. Stoops may be recessed into the building façade.</li> <li>2. Stoop may be built perpendicular or parallel to the building face, but must lead directly to ground level or an abutting sidewalk from the building entrance.</li> <li>3. Stoops may be configured as a split stair to access a below grade unit.</li> <li>4. If a stoop is built encroaching onto a sidewalk, it must provide at least 5 ft clear and unobstructed between its outermost face and the face of the curb.</li> </ol>		

4. **Parking Standards.** Parking areas in the BSN shall meet the following criteria:

A. **Parking location**

- Parking shall be located to the rear and side of a building where physically feasible.
- Where parking is located on the side of a building and visible from the street or adjacent properties, it shall be screened through densely planted shrubbery or fencing, which in the opinion of the Planning Board effectively screens the parking and is of a quality that positively contributes to the character of the neighborhood.

**B. Parking ratios**

- i. Residential uses shall have 1.0 spaces per unit
- ii. Commercial uses shall have 4 spaces per 1,000 sf of leasable floor space
- iii. Other uses shall have 3 spaces per 1,000 sf of net floor area

**C. Parking requirements reduction**

- i. The Planning Board may grant a waiver that allows a decrease in the required number of parking spaces through consideration of the following:
  - i. On-Street Parking Off-Set. For commercial uses parking spaces required to meet the minimum off- street parking requirements may include publicly available on- street parking spaces along the building lot frontage on the same side of the street for non-residential uses.
  - ii. Shared Parking and Mixed Use.
    - 1. A combination of uses on-site using shared parking lots with offset peak demand times where: a shared parking agreement with onsite uses where uses have offset peak demand times; uses have a high rate of parking turnover; or evidence of similar uses and location situations operating successfully with lower amounts of parking.
    - 2. In commercial or mixed-use developments where shared parking is proposed, the Planning Board may require an evaluation prepared by the applicant following the procedures of the Urban Land Institute (ULI) Shared Parking Manual (latest edition) or the Institute of Transportation Engineers (ITE) Shared Parking Guidelines (latest addition), or other approved procedures determined by the Planning Board.
  - iii. Car-Sharing Program. The Planning Board may approve a parking reduction where an active car-sharing program is made available to residents and/or employees of a development Site; and where cars for the car-share program are available on the site or within a 700-foot walking distance of the site.

**5. Screening requirements**

**A. Loading Facilities.**

- i. Outdoor loading facilities, including all docks and areas used for the storage and staging of good or materials must be screened from view with landscaping or by a wall or fully closed fence, as necessary to sufficiently screen delivery vehicles. If a

- wall or fence is used it must be constructed of materials that are compatible with the building in terms of texture, quality, and color.
- ii. Exterior entrances and access to loading facilities that are fully enclosed within a building must have an opaque, self-closing door constructed of materials that are compatible with the principal building in terms of texture, quality, and color.
- B. Service Areas**
- i. Trash collection, trash compaction, recycling collection and other similar service areas must be fully enclosed within a building or located to the side or rear of the buildings and screened appropriately.
  - ii. Exterior entrances and access to service areas that are fully enclosed within a building must have an opaque, self-closing door constructed of materials that are compatible with the principal building in terms of texture, quality, and color.
  - iii. Outdoor service areas that are visible from a public street, public space, or abutting properties in the Residence district must be fully screened by a wall or fully closed fence at least six (6) feet in height with self-closing access doors and constructed of materials that are compatible with the principal building in terms of texture, quality, and color.
- C. Mechanical Equipment**
- i. Roof-mounted mechanical equipment, except sustainable energy systems, and elevator or stairwell penthouses must be set back at least ten (10) feet from the exterior walls of a building.
  - ii. New buildings must provide a parapet wall or other architectural element constructed of materials that are compatible with the building in terms of texture, quality, and color that screens the visibility of roof-mounted mechanical equipment from any a public street or public space.
  - iii. Wall-mounted mechanical equipment on any surface that is visible from a public thoroughfare (excluding an alley) or civic space must be screened by landscaping or an opaque screen constructed of materials that are compatible with the principal building in terms of texture, quality, and color.
  - iv. Ground-mounted mechanical equipment that is visible from a public street or public space must be screened by landscaping, a fence, or a wall constructed of materials that are compatible with the principal building in terms of texture, quality, and color.
  - v. Vents attached to a wall must not be located along the façade of the Primary Street and should be painted to blend in with the color of the building façade upon which it is located.
  - vi. Screening must be of a height equal to or greater than the height of the mechanical equipment being screened.
  - vii. An acoustic study of any relevant mechanical equipment on the effects of abutting properties and the public realm shall be included in the application materials.

**8.7.9 Administrative Plan Review Application.** Any application for approval of an administrative plan review under this section shall be accompanied by the application requirements of Section 9.5.

**8.7.10 Site Plan Review Application.** In addition to the application requirements of Section 9.5, the site plan review shall include a narrative of how the project complies with the criteria set forth in Section 8.7.13.

**8.7.11 Development Standards Waiver Special Permit Application.** Such application shall be accompanied by a narrative that describes of how the project complies with the criteria in Section 8.7.14, despite the request for said Waiver set forth.

**8.7.12 Administrative Plan Review Criteria.** The Planning Director or their designee shall review all such submitted plans in accordance with the development standards established in Section 8.7.8.

**8.7.13 Site Plan Review Criteria.**

1. The Planning Board shall review all such submitted plans in accordance with the design guidelines herein and the following criteria:
  - A. The development standards established in Section 8.7.8. unless otherwise waived by special permit from the Planning Board.
  - B. Adequacy of the building layout in a way to create a presence on main corridor street edges.
  - C. Adequacy of the parking facility to be located to the rear or side of the structures whenever physically feasible and adequacy of avoiding large expanses that are unbroken by buildings or substantial landscaped areas.
  - D. Adequacy of the pedestrian connection to streets and pedestrian ways.
  - E. Adequacy of traffic calming measures that discourage cut through traffic in the rear parking lot of any site.
2. The Design Review Board shall review all such submitted plans in accordance with the design guidelines herein and the following criteria:
  - A. Adequacy of the design to complement and harmonize with adjacent land uses (existing and proposed) with respect to architecture, scale, landscaping, and screening.
  - B. Adequacy of the use of building materials and details that are consistent with the architectural heritage of surrounding buildings.
  - C. Adequacy of new facade elements on existing buildings to use materials and details that are consistent with the architectural heritage of the buildings on which they are located.

**8.7.14 Development Standards Waiver Special Permit Criteria.** In the Bridge Street Neck Overlay, no development standard waiver shall be approved unless all the following



criteria, in lieu of those set forth in Section 9.4, are found to exist as part of the granting of a special permit by the Planning Board:

1. The Design Review Board makes a positive recommendation that the deviations from the standards and guidelines contained herein would improve the overall design of the proposed project.
2. The Planning Board finds that the overall design would be more compliant with the intent and purpose of the ordinance and the Bridge Street Neighborhood Vision Update (2020 or most recent edition) than a strict adherence to the guidelines.

**8.7.15 Administrative Plan Review Action.** The Planning Director or their designee shall approve the plan provided that the plan complies with the review criteria herein. If a project does not comply with the review criteria, the applicant shall be directed to revise the plan or apply for a Site Plan Review and Development Standard Waiver Special Permit.

**8.7.16 Site Plan Review Action.** The Planning Board shall not take final action on the application until it receives written recommendation on the application from the Design Review Board of the Salem Redevelopment Authority.

**8.7.17 Development Standard Waiver Special Permit Action.** Action shall not be taken on any plan for a Site Plan Review until it has received a positive recommendation from the Design Review Board of the Salem Redevelopment Authority concerning the design of the project. The Design Review Board shall refer to the Development Standards and Guidelines herein and the Bridge Street Neck Vision Update Plan (2020 or most recent edition) when making their recommendation.

1. The Design Review Board's recommendation shall state the applicable standard(s), whether the alternative design approach meets the Design Guidelines and if a waiver is recommended.
2. The Planning Board must provide a written determination and finding whether the alternative design approach meets the requirements of the Design Criteria. Such determination shall state the applicable standard(s), the reason for granting an alternative, the applicable Design Criteria, and how the alternative meets the Design Criteria.

**8.7.18 Administrative Plan Review Appeal.** Any appeal of a decision of the Planning Director pursuant to this section shall be appealed to the Planning Board.

**8.7.19 Site Plan Review Appeal.** Any decision of the Board pursuant to this section shall be appealed in accordance with G.L. c. 40A, s. 17 to a court of competent jurisdiction.

**Section 2. Amend Section 2.2 Overlay Districts by inserting the following district:**

Bridge Street Neck Overlay District	(BSN)
-------------------------------------	-------

**Section 3. Amend Section 3.1 TABLE OF PRINCIPAL AND ACCESSORY USE REGULATIONS by inserting three asterisks to Planned Unit Development with the following table note.**

\*\*\*Planned unit development is not permitted in the Bridge Street Neck Overlay District.

TABLE OF PRINCIPAL AND ACCESSORY USE REGULATIONS											
PRINCIPAL USES	RC	R1	R2	R3	B1	B2	B4	B5	I	BPD	NRCC
<b>A. RESIDENTIAL USES</b>											
Planned unit development***	N	N	N	PB	PB	PB	PB	PB	PB	Y	-

**Section 4. This Ordinance shall take effect as provided by City Charter.**

In City Council March 10, 2022

Referred to the Planning Board to hold a Joint Public Hearing by a roll call vote of 11 yeas, 0 nays and 0 absent

In City Council April 13, 2022

Joint Public Hearing closed by a roll call vote of 10 yeas, 0 nays and 1 absent

Referred to the Planning Board for recommendation by a roll call vote of 10 yeas, 0 nays and 1 a

ATTEST:

ILENE SIMONS  
CITY CLERK

## CITY OF SALEM

In the year Two Thousand and Twenty-Two

**An Ordinance** to amend language relative to the senior resident tax work off abatement program.

**Section 1.** Section 2-2052 *Program guidelines* of Chapter 2 Administration is hereby amended in subsection (a) by deleting the number "\$1,000.00" and replacing it with the number "\$1,500.00".

**Section 2.** Section 2-2052 *Program guidelines* of Chapter 2 Administration is hereby amended in subsection (d) by deleting the number "30" and replacing it with the number "60".

**Section 3.** This Ordinance shall take effect as provided by City Charter.

In City Council April 14, 2022  
Adopted for first passage

ATTEST:

ILENE SIMONS  
CITY CLERK

## CITY OF SALEM

In the year Two Thousand and Twenty-Two

**An Ordinance** to ensure a more inclusive municipal Code of Ordinances.

**Section 1.** Section 2-532 – *Qualifications* of Chapter 2 is hereby amended by replacing the word “citizen” with the word “resident”.

**Section 2.** Article XV. – *Senior Citizen Tax Work Off Abatement Program* of Chapter 2 is hereby amended in the Article title by replacing the word “Citizen” with the word “Property-Owner”

**Section 3.** Section 2-1733 – *General duties* of Chapter 2 is hereby amended by replacing the word “citizen” with the word “individual”.

**Section 4.** Section 14-402 – *Issuance of licenses* of Chapter 14 is hereby amended by replacing the word “citizens” with the word “residents”.

**Section 5.** Section 24-4 – *Discharging firearms* of Chapter 24 is hereby amended by replacing the word “citizen” with the word “individual.”

**Section 6.** Section 26-66 – *Purpose and goals* of Chapter 26 is hereby amended in subsection (a) by replacing the word “citizens” with the word “residents” in both instances in which it appears in the subsection.

**Section 7.** Section 30-30 – *General duties* of Chapter 30 is hereby amended by replacing the word “citizens” with the word “residents.”

**Section 8.** Section 32-19 – *Constables* of Chapter 32 is hereby amended in subsection (b) by replacing the word “citizens” with the word “residents”.

**Section 9.** Section 32-130 – *Preamble* of Chapter 32 is hereby amended by replacing the word “citizens” with the word “residents”.

**Section 10.** Section 32-122 – *Legislative findings* of Chapter 32 is hereby amended in subsection (a) by replacing the word “citizen” with the word “individual” and by adding at the end of subsection (a) “to the extent permitted by law.”

**Section 11.** This Ordinance shall take effect as provided by City Charter.

In City Council April 14, 2022  
Adopted for first passage

ATTEST:

ILENE SIMONS  
CITY CLERK

**CITY OF SALEM**

In the year Two Thousand and Twenty-two

An Ordinance to amend an Ordinance relative to Traffic – Handicap Parking

Be it Ordained by the City Council of the City of Salem, as follows:

**Section 1.** Chapter 42, Section 50B – “Handicap Parking – Time Limited” is hereby amended by adding:

Symonds Street – in front of #16 Symonds Street for a distance of twenty (20) feet

**Section 2.** This Ordinance shall take effect as provided by City Charter.

In City Council April 14, 2022  
Adopted for first passage

ATTEST:

ILENE SIMONS  
CITY CLERK

**CITY OF SALEM**

In the year Two Thousand and Twenty-Two

An Ordinance Relative to Traffic

*Be it ordained by the City Council of the City of Salem, as follows:*

**Section 1.** Article I is hereby amended by repealing the following to Section 17A:

Section 50 - Prohibited in certain specified places. Within Ten Feet of a Fire Hydrant \$50

Section 50 - Prohibited in certain specified places. Upon a Crosswalk \$25

Section 50 - Prohibited in certain specified places. Upon a sidewalk \$25

Section 50 - Prohibited in certain specified places. Within Four Feet of Driveway or Private Road \$15

Section 52. - Bus stops. Bus Stop \$15

Section 75 - \$15.00 Fine/\$40.00 Fine on Friday, Saturday and Sunday during Month of October for violation of Parking Prohibitions Towing Zone (Resident Sticker)

Section 75A – Parking Prohibitions towing zone (October Resident Parking) – \$50

And replacing it with:

Section 50 - Prohibited in certain specified places. Within Ten Feet of a Fire Hydrant \$50.00 Fine/\$75.00 Fine during Month of October

Section 50 - Prohibited in certain specified places. Upon a Crosswalk \$25.00 Fine/\$75.00 Fine during Month of October

Section 50 - Prohibited in certain specified places. Upon a sidewalk \$25.00 Fine/\$75.00 Fine during Month of October

Section 50 - Prohibited in certain specified places. Within Four Feet of Driveway or Private Road \$15.00 Fine/\$75.00 Fine during Month of October

Section 52. - Bus stops. Bus Stop \$15.00 Fine/\$75.00 Fine during Month of October

Section 75 - \$15.00 Fine/\$75.00 Fine during Month of October for violation of Parking Prohibitions Towing Zone (Resident Sticker)

Section 75A – \$75.00 Fine Parking Prohibitions towing zone (October Resident Parking)

**Section 2.** This ordinance shall take effect as provided by City Charter.

In City Council April 14, 2022  
Adopted for first passage

ATTEST:

ILENE SIMONS  
CITY CLERK



**CITY OF SALEM**

In the year Two Thousand and Twenty-two

An Ordinance to amend an Ordinance relative to Traffic – Handicap Parking

Be it Ordained by the City Council of the City of Salem, as follows:

**Section 1.** Chapter 42, Section 50B – “Handicap Parking – Time Limited” is hereby amended by adding:

Hersey Street – in front of #18 Hersey Street for a distance of twenty (20) feet

**Section 2.** This Ordinance shall take effect as provided by City Charter.

In City Council April 14, 2022  
Adopted for first passage

ATTEST:

ILENE SIMONS  
CITY CLERK

**CITY OF SALEM**

In the year Two Thousand and Twenty-two

An Ordinance to amend an Ordinance relative to Traffic – Handicap Parking

Be it Ordained by the City Council of the City of Salem, as follows:

**Section 1.** Chapter 42, Section 50B – “Handicap Parking – Time Limited” is hereby amended by adding:

Chase Street – in front of #14 Chase Street for a distance of twenty (20) feet

**Section 2.** This Ordinance shall take effect as provided by City Charter.

In City Council April 14, 2022  
Adopted for first passage

ATTEST:

ILENE SIMONS  
CITY CLERK